

Putting the Plan to Work



I Priorities and Next Steps

The Design Team Report describes a series physical design initiatives for the revitalization of the JeffVanderLou neighborhood. Collectively these design initiatives make up the neighborhood vision or Master Plan (as illustrated by the drawing on page 9). The Master Plan, however, does not describe an end product that is to be constructed in its entirety. Rather, the Plan should be viewed as a "menu" or "shopping list" of individual initiatives, which can be prioritized and implemented over time. The advantage of this type of plan is that it is flexible, allowing the neighborhood to select the highest priority initiatives at the beginning, and to delay the implementation of lower priority initiatives until additional funds become available. The initiatives are designed to build on the neighborhood strengths and assets and to eliminate weaknesses and problems. The implementation of any single initiative therefore works towards the overall goal of neighborhood revitalization.

In addition to being flexible, the Master Plan illustrates a clear vision of what the neighborhood can be in the future and will act as a powerful tool for attracting private development interest and funding. Unlike plans that rely exclusively on public funds for implementation, this initiatives plan is designed to use limited public investment in strategic areas as a means of attracting private development dollars. A cost breakdown of key public infrastructure costs (for streetscaping and parks) is set out in Appendix C. Building on the already substantial investment in the Vashon High School, modest investments in new landscaped streets and parks will help to attract current and former residents to build new homes and renovate old ones, and developers to invest in the revitalization of entire blocks or series of blocks. Again, the Plan should be seen as a means of attracting investment, rather than a blueprint for spending money.

Appendix A: Design Guidelines



Beaux Arts Classical

This style has its roots in the tradition of Classical building as reintroduced in this country at the Chicago World Exposition in 1893. The style had great influence on the popular domestic architectural taste of the time. St. Louis' finest urban neighborhoods have houses and apartment buildings designed in the Classical style. Classical massing typically involves simple rectangular volumes, topped by a hipped, gable or flat roof, with broad cornices and eaves accented by classical mouldings.

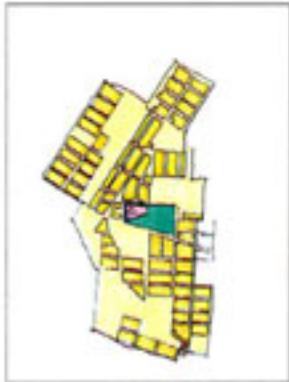
Classical porches typically have either single or paired, round or square, classical columns and details with railings and turned balusters. Porch roofs are hipped, shed (commonly with a pediment above the door), or flat.

Standard windows are 3-feet wide double-hung with a 6 over 6 or 6 over 9 divided light pattern. First floor windows are 6 feet 2 inches tall; second floor windows are 5 feet 6 inches tall. Front doors are typically 6 or 8 panel with sidelights and transoms.



Beaux Arts Classical house in North St. Louis

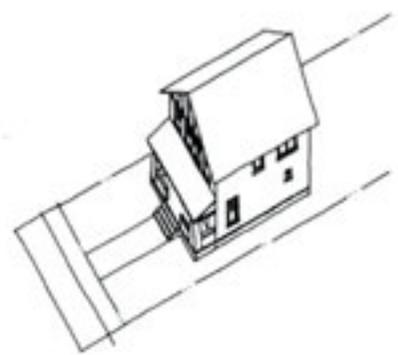
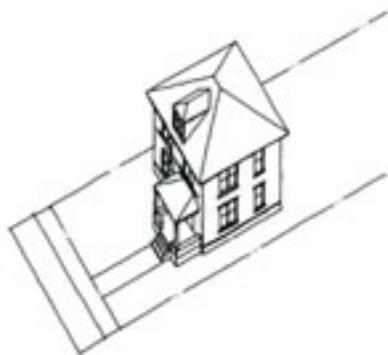
I Design Guidelines



THE JEFFVANDERLOU AREA and its surrounding North St. Louis neighborhoods have a strong architectural heritage. As revitalization begins to take shape, it is important to have a set of standards to guide the form and character of the many different efforts. Participants in the planning process have clearly stated their desire to see the rehabilitation of historic homes and new development that builds on the traditional and historic quality of the inherited building fabric. The distinctiveness of the neighborhood must remain as new development occurs. It is, therefore, important to renovate as many structures as is economically feasible and build new buildings that fit into the established patterns. This fit includes the way houses sit on their lots, the size of the lots, the massing and architectural character of the new houses, and the materials and colors used.

A-1

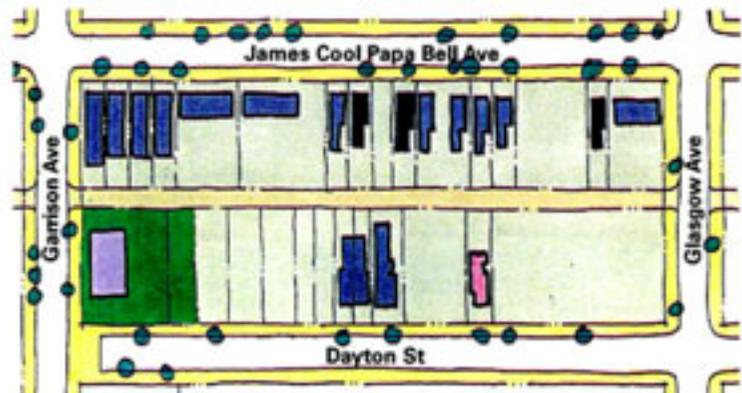
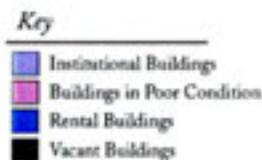
This section outlines a series of design guidelines for rehabilitation and infill development within the JeffVanderLou area. The first part deals with the overall strategy for revitalizing a typical block with new infill construction and rehabilitation. The standard lot sizes and building placement for various housing types are then described. Third, architectural guidelines for preserving the neighborhood's architectural character with new infill development are discussed, with examples of typical elevation details provided as illustrations of the guidelines.



II Prototype Block Revitalization Strategy

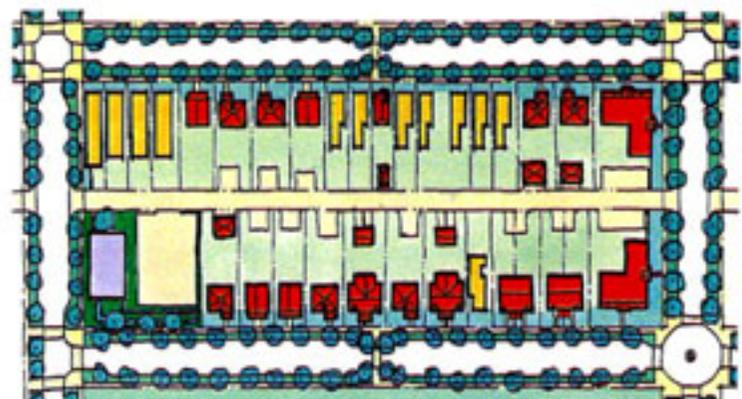
- Acquire all vacant lots and buildings.
- Acquire problem absentee-owned properties.
- Renovate or demolish vacant buildings and absentee-owned buildings unsuitable for rehabilitation and build infill housing on vacant lots.
- Renovate suitable absentee landlord properties as either owner-occupied townhouse or condominium units, or as well-managed rental apartments.
- Help homeowners living in homes with deferred maintenance needs to find financial assistance to repair their residences.
- Install traditional residential scale street lighting.
- Plant street trees and grass in tree lawns adjacent to curb.
- Calm traffic with landscape islands or curb bump-outs at intersections.
- Add new cross-streets where blocks are too long.

A-2



Typical Block Revitalization

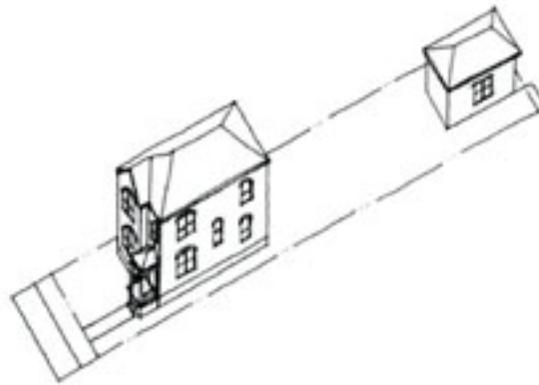
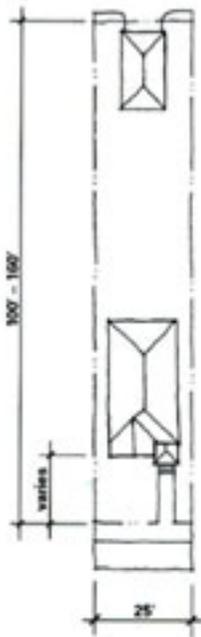
Plan before (above right) and after (right) of the block just north of Seay Park, showing typical revitalization strategies including infill, renovation, and street improvements.



III Lot Prototypes: Residential

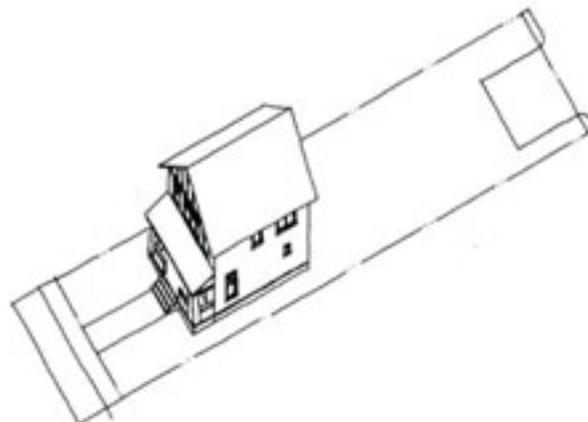
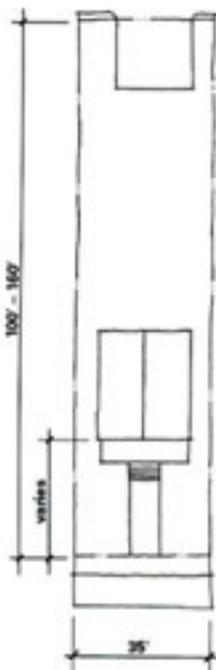
LOT PROTOTYPES FOR infill construction are based on both the historic lot patterns found in the JeffVanderLou neighborhood and the sizes of the anticipated new building types. These reflect current market standards for urban infill housing in many urban centers, taking advantage of the inherited form of the neighborhood and the ability to service lots from alleys, where all units can have either a garage or parking pad. Front setbacks on all new infill buildings should be consistent with the surrounding buildings.

A-3



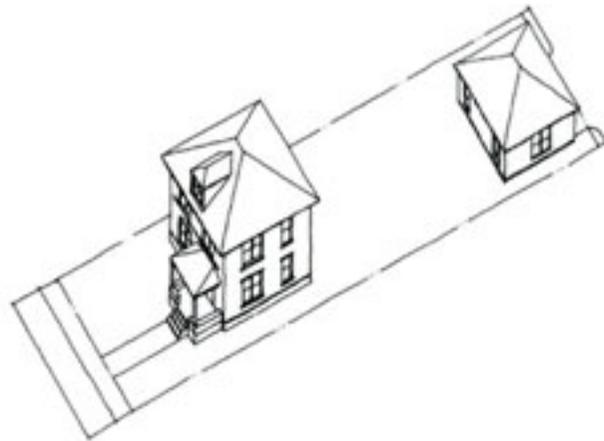
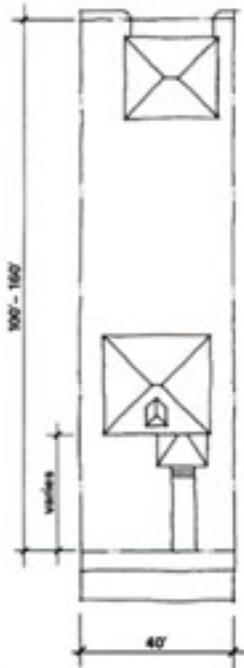
A. Single-Family Detached

20-foot wide, two-story single family detached house on a 25-foot lot



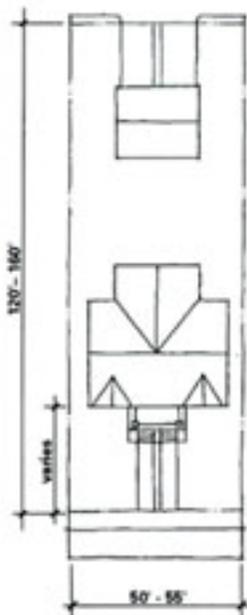
B. Single-Family Detached

24- to 26-foot wide, two-story single family detached house on a 35 foot lot



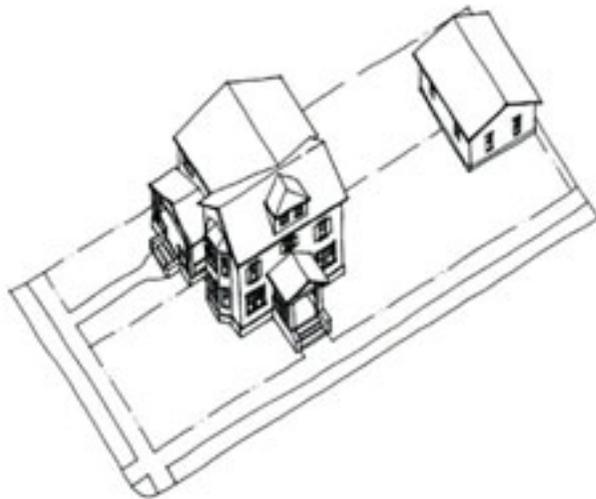
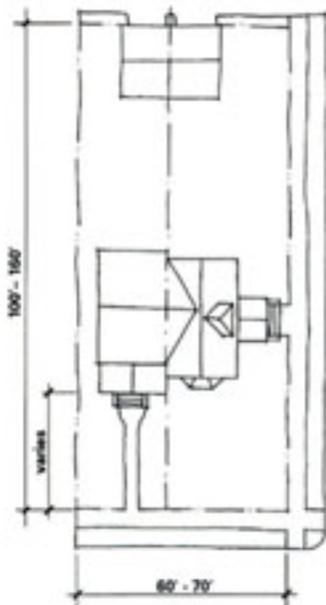
C. Single-family Detached

28- to 30-foot wide, two-story single family detached house on a 40-foot lot



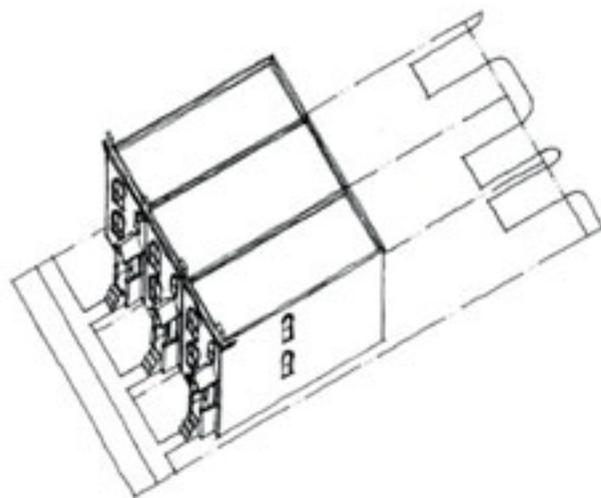
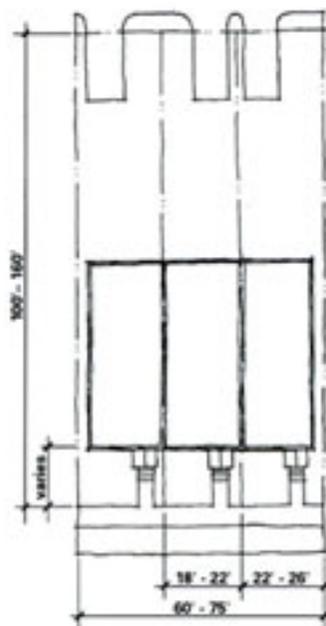
D. Mid-Block Duplex

36- to 44-foot wide buildings on 50- to 55-foot wide lots



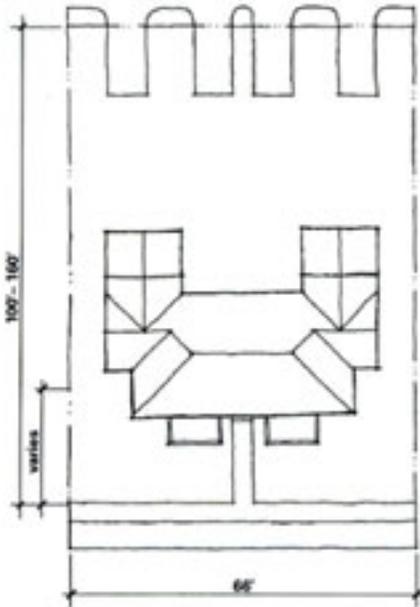
E. Corner Duplex

36- to 44-foot wide buildings on 60- to 70-foot wide corner lots. One unit has a front yard on the side street.



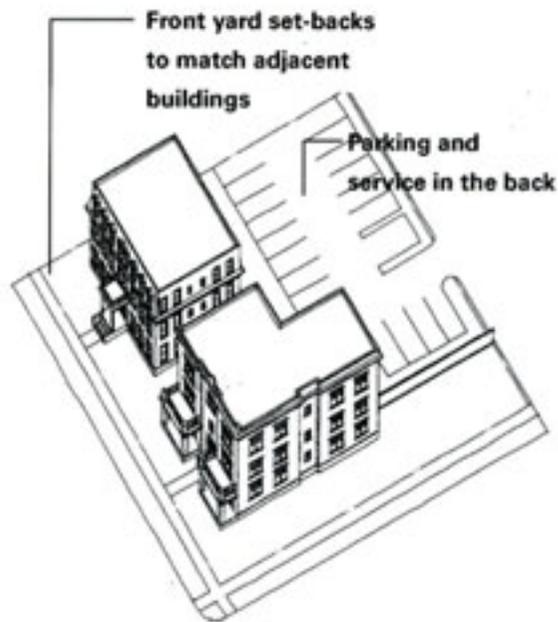
F. Mid-Block Triplex

3, 18- to 22-foot wide attached townhouse units on 18- to 26-foot wide lots



G. 4-Unit Apartment Building

56-foot wide 4-unit apartment building, with 2 units per floor, on a 66-foot wide lot



H. Apartment Buildings

Mid-block and corner apartment buildings

IV Lot Prototypes: Mixed-Use and Commercial Buildings



Commercial buildings existing on Grand Avenue serve as precedents for new commercial development.

WHILE RESIDENTIAL DEVELOPMENT is proposed for most of the JeffVanderLou area, mixed-use and commercial rehabilitation and development are appropriate along the thoroughfares that have historically accommodated the neighborhood's retail. Boulevard frontage along Grand Boulevard and the small green squares on Dr. Martin Luther King Jr. Drive provide ideal addresses for this type of development. Typically these buildings are two to three stories high and fit within the standard 140-foot deep lot. Buildings should form a continuous perimeter on the blocks with off-street parking and service areas in the rear, hidden from public view, wherever possible. Parking can also be accommodated on the street and in parking lots that are screened from view with landscaping and walls that maintain a pedestrian-friendly street frontage.

A-7

In mixed use buildings, the massing and facade treatment should emphasize a first floor retail base with storefront bays.



v Architectural Guidelines



St. Louis architectural precedent.

Typical Street Elevation

Infill development, consistent in character with the neighborhood's historic homes, creates a coherent streetscape.



JEFFVANDERLOU RESIDENTS want the integrity of the historic neighborhood to be preserved as new housing is constructed. Residents expressed concern that some of the new construction occurring in and around the neighborhood is not complementary to the inherited stock of early twentieth century houses. While many of the historic houses are more narrow and deep than what might be built today, the patterns of architectural style, massing and materials can become the guiding principles for the design of new infill housing. This plan proposes that the new housing be designed in one of three principal styles found in and around the neighborhood. These principal existing styles are Beaux Arts Classical, Arts & Crafts and Second Empire Victorian .

A-8

The composition of architectural elements in the recreation of each style and the uses of materials will be key to blending into the existing neighborhood.

Even though much of St. Louis is built in distinctive red brick, many neighborhoods developed in an Arts & Crafts style, using siding and frame materials to complement basic brick structures. When materials are mixed in new construction, they must be combined in a way that is faithful to the neighborhood's stylistic traditions. While residents would understandably prefer that any new buildings be constructed entirely of brick, economic concerns may require the use of other materials on some parts of the structure. In this case, front facades for mid-block houses can be brick while other facades use a less expensive material. Corner houses should always use consistent materials on each street-facing elevation.

Each of the styles discussed in this report can be found within the planning area and constitute the fabric of neighborhood streets. The following pages summarize the key elements for each style.

It also is important in planning new residential construction to consider accessibility for people with mobility impairments. While the "senior cottages" described in this section will be the most accessible of the building types recom-

mended for new construction, accessibility is also important in designing within the other more traditional styles discussed. Universal design, which makes a home accessible to senior citizens and children as well as people of all ages with disabilities, should be considered for some new homes. Concern for accessibility and visitability should be balanced with concern for creating a streetscape where all of the building types fit comfortably together.



Historic Infill and Rehabilitation

A street with many vacant lots (right) is transformed into a pleasant residential street with infill housing (above).



Beaux Arts Classical

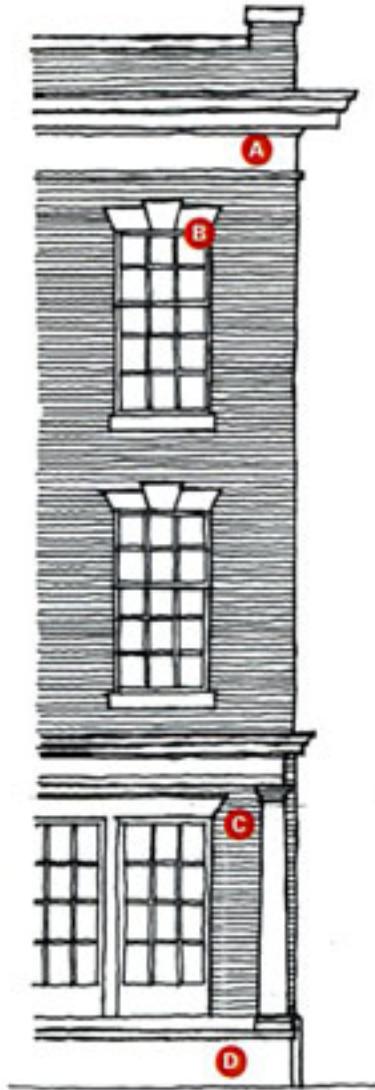
This style has its roots in the tradition of Classical building as reintroduced in this country at the Chicago World Exposition in 1893. The style had great influence on the popular domestic architectural taste of the time. St. Louis' finest urban neighborhoods have houses and apartment buildings designed in the Classical style. Classical massing typically involves simple rectangular volumes, topped by a hipped, gable or flat roof, with broad cornices and eaves accented by classical mouldings.

Classical porches typically have either single or paired, round or square, classical columns and details with railings and turned balusters. Porch roofs are hipped, shed (commonly with a pediment above the door), or flat.

Standard windows are 3-feet wide double-hung with a 6 over 6 or 6 over 9 divided light pattern. First floor windows are 6 feet 2 inches tall; second floor windows are 5 feet 6 inches tall. Front doors are typically 6 or 8 panel with sidelights and transoms.



Beaux Arts Classical house in North St. Louis



Essential Elements include:

- Simple, well proportioned volumes with consistent roof pitches.
- Well detailed Classical eaves and cornices (A).
- Wide window trim with a decorative crown or lintel with keystone (B).
- Entry porches with roofs supported by Classical columns (C).
- An orderly relationship among windows, doors, porches and roof forms.
- Foundations differentiated from the rest of the structure which raise the ground floor to several steps above grade (D).

Second Empire Victorian

The Second Empire style was dominant between 1860 and 1900 when St. Louis' early urban neighborhoods were built, and was based on the latest French building fashions at the time. This style is still found in North St. Louis, and can be used to add variety to the neighborhood's fabric, particularly where a larger townhouse or apartment house scale is needed.

Second Empire massing is a simple rectangular three-story volume. The third story is contained in a slate-tiled mansard roof with dormers on the street facades, supported by a deep cornice, often with brackets.

Second Empire construction was historically brick, often faced with limestone on the foundation and primary elevations. Today, stucco can be scored to resemble limestone.

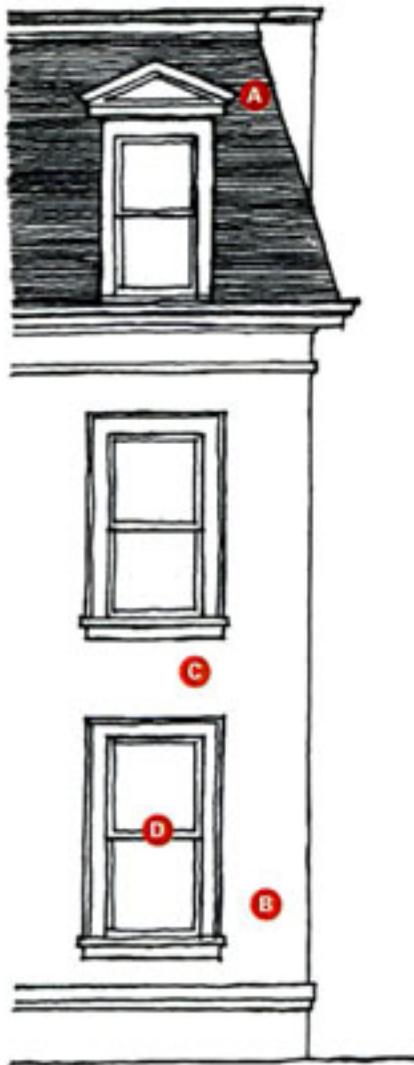
Instead of front porches, Second Empire structures have recessed entries to provide shelter from the elements. Rear

porches are common; side porches often grace the more elaborate examples of this style. Columns for these porches are often constructed of single cuts of wood, gracefully turned. Porch roofs are either flat or shed; when flat, they can provide a balcony for the upper floors and are finished with a decorative wood or metal balustrade.

Standard windows are 2 feet-8 inches wide double-hung, usually with a 1 over 1 configuration on the primary elevations and 2 over 2 elsewhere. First floor windows are typically 6 feet-2 inches tall, with second floor windows in the 5-foot-6-inch range. Window trim is typically a 2-inch x 2-inch profiled molding recessed 1 to 2 inches into the wall opening, although some buildings have 6-inch profiled mouldings. On bay windows, the structural wood elements supporting the window assemblies have wider and more ornate details.



Second Empire Victorian house in North St. Louis



Essential Elements include:

- Simple, three-story volumes, with the ground floor raised several steps above grade.
- Steeply pitched mansard roofs with dormers and deep cornices at the street facade. The pitch of the mansard roof is particularly important in achieving a true Second Empire look (A).
- Stucco 'limestone look' foundations on the front elevation, appropriately finished and scored (B).
- An orderly relationship among windows, doors, porches and roof forms.
- Recessed doorways without porches on the front elevation.
- Facades of brick masonry or stucco finished and scored to resemble the traditional limestone (C).
- Tall, well-proportioned windows with appropriately profiled recessed trim (D).
- Wood trim and stucco painted in colors which accent the structure's design.

St. Louis Arts & Crafts

St. Louis has a significant inventory of Arts & Crafts houses, built between 1905 and 1930, and designed in the Craftsman, Four Square and Prairie styles. Single-story, one-and-one-half, and two-story detached structures, as well as townhouses and apartment buildings can be found. Arts & Crafts massing is characterized by simple rectangular volumes with front-facing gable roofs and wide overhanging eaves, or, in the case of townhouses and apartment buildings, flat roofs with false front gables or bracketed cornices.

One- and two-bay porches are common, with the porch columns being a

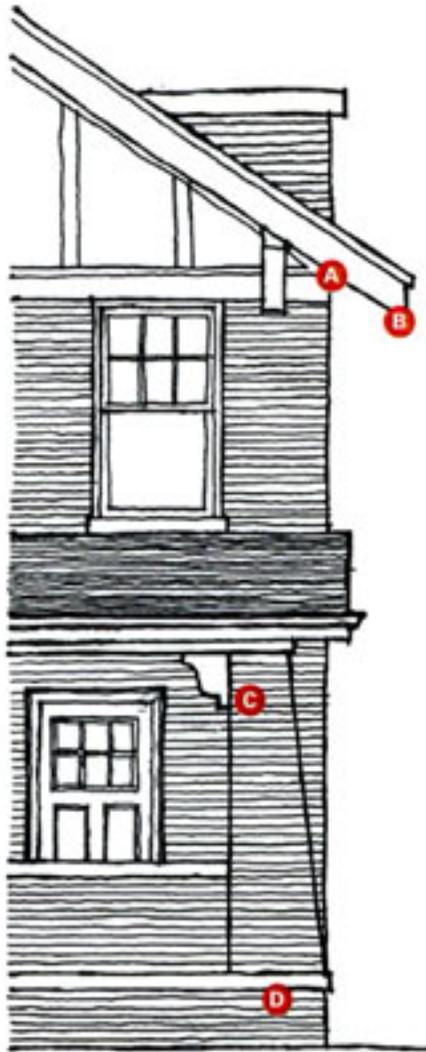
distinctive element of the style. They often have sloping sides and extend from the ground to above the porch floor without a break, and are of varied materials, such as brick, stone, stucco and wood, often in combination. Porch roof forms are typically front gable, shed or flat.

Standard windows are double-hung with a 4, 6 or 8 over 1 divided light pattern. First floor windows are 6 feet 2 inches tall; second floor windows are 5 feet 6 inches tall. Front doors often have fanciful geometric divided light patterns.

This style provides the basis for one of the 'senior cottage' design which follow on page A-16.



Arts & Crafts houses in the North St. Louis area



Essential Elements include:

- Simple, well proportioned volumes with gabled roof.
- Deep overhanging eaves at sides and front, often with ornamental brackets (A).
- Ornamental half-timbering at gables (B).
- Grouped windows in pairs or in threes with upper sash divided light patterns.
- Deep porches with brick piers, often tapered, or wood columns on brick piers (C).
- Masonry or fieldstone foundations visually distinguished from the upper structure by material or detailing. The first floor level is usually several feet above grade (D).



Senior Cottage Units

The subcommittee participants expressed significant interest in providing housing for seniors. While the one story unit type does not fit into the fabric of the larger two story precedents found on most streets in the neighborhood, these houses can work as courts or private place developments much like Fountain Place in North St. Louis. These duplex units require a 90 - 100 foot wide lot and are serviced by an alley. The accessible entrances are from the porch adjacent to the alley, and the front is slightly raised on the street side.



Senior Cottages

These duplex buildings can be designed in a variety of styles, such as Colonial Revival (bottom) and Arts & Crafts (top). The typical floor plan at left shows two attached 750 square foot, one bedroom units.

Landscape Recommendations

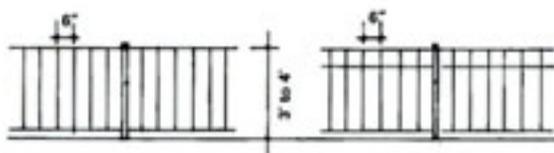
As a minimum requirement, all new residential structures should have seeded lawns around the entire house and foundation plantings along their front facades. Landscape plantings and treatment help to define the mood for the residence as well as that of the entire neighborhood. While landscaping generally enhances property, it also has a functional purpose of providing privacy and possibly shading.

Whenever possible, plant materials that are native to Missouri are encouraged for use on individual house lots. Native plants generally require less extensive maintenance and help create the character of place that is unique to this region. Native plants will be used extensively in parks and public open spaces where there is sufficient room to plant them as they would naturally occur in a *plant community*. See the list of preferred plants on page A-20.

Fencing

Front and Side Yard Fencing is recommended primarily on corner lots adjacent to the sidewalk. These fences should be a transparent style with metal pickets not to exceed 36 to 42 inches in height. Brick or stone piers are encouraged at 90-degree transitions and street intersections. Front and side yard fences should be located 12 to 24 inches from the sidewalk with grass, a hardy ground cover, or a low hedge planted between.

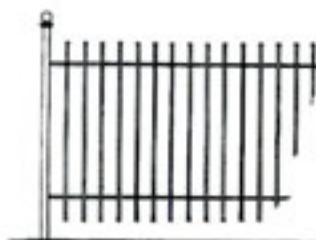
Front Yard / Back Yard Fencing is recommended to extend between a building and an alley or garage and between adjacent buildings. These fences shall be a transparent style with pickets 36 to 48 inches in height. Low hedges may be planted to the street side of these fences to soften their appearance.



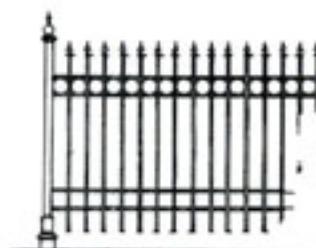
Front yard / side yard fencing with prefinished metal



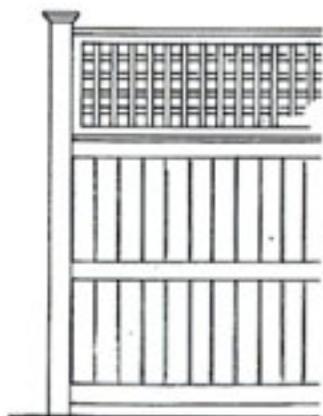
Front yard / side yard fencing: corner condition



Simple iron fencing



Ornate iron fencing



Screening (privacy fencing)

Screening (Privacy Fencing) is not part of the historic JeffVanderLou vocabulary, but will be required around dumpsters. These fences should be opaque to a minimum height of 48 inches with 24 inches of transparent fencing (lattice) above. Privacy fencing should be stained or painted a very dark color to blend in with the surrounding landscape.

Accessibility and Visitability

Great care should be taken to eliminate physical barriers in the neighborhood such as slopes and their associated steps. Outdoor stairs in the public right-of-ways are discouraged. Buildings open to the public will be entered at grade and will be characterized by barrier-free design.

Achieving a visitability standard across the neighborhood is also an important goal. Whenever possible, one entrance to a home should be at grade, approached by a paved accessible route. All interior passage doors should be a minimum of 2 feet 10 inches wide and all units should have a first floor bathroom.

Designers should refer to the following resources when designing for accessibility: The Architectural Barriers Act (1968), Section 504 of the Rehabilitation Act (1973), The Fair Housing Act of 1968 (as Amended), The Americans with Disabilities Act (1990), ANSI and UFAS, and all applicable state and local codes.

Mobility Impaired Ramps

When mobility impaired ramps are necessary, they should be attached to the back or side of a residence. An accessible surface parking pad should be located adjacent to the ramp. Ramps that are designed or placed in such a way that they announce that the resident is disabled represent a subtle form of discrimination and should be avoided. Side ramps that are visible from the street and should incorporate the same architectural vocabulary as front porches. Two sets of rails should be provided—a grab rail parallel to the ramp to assist the user, and a level handrail to relate the addition to its context.

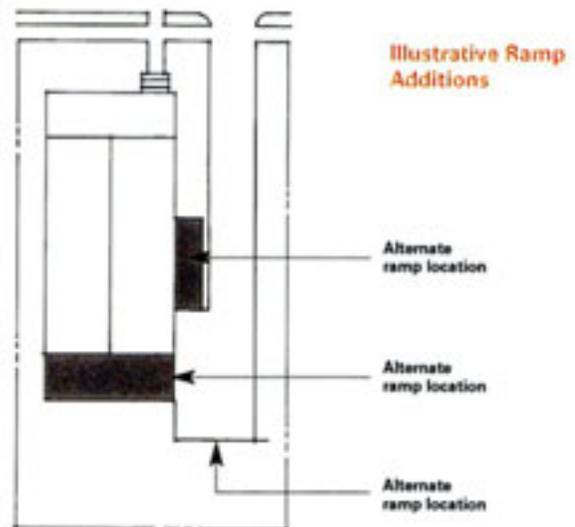
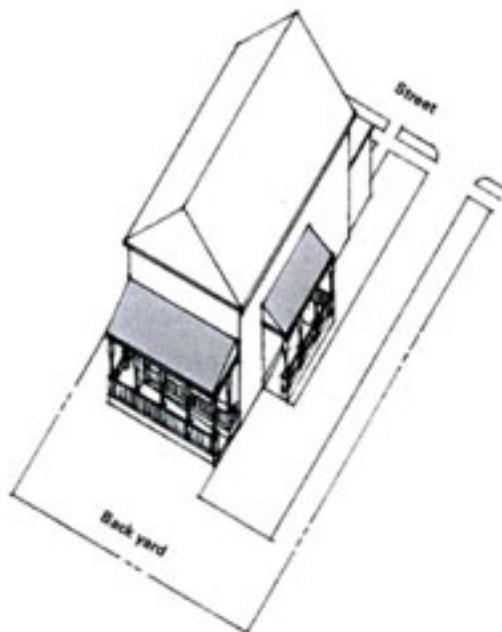
Residential Lighting

Residential lighting outside the public right-of-way should be the minimum

required to provide at least some illumination on all walks and porches. Decorative, dark colored lamp posts, compatible with the architecture of the house, are encouraged on single-family lots. Similar porch lights or wall lanterns are encouraged on all buildings.

Ancillary Structures and Mechanical Units

Ancillary structures, such as garages or trash enclosures, are to be integrated into the landscape and screened from public view with shrubs, hedges, fences, walls, or a combination of these elements. Trash enclosures and screening should blend in with the landscape and surrounding environment. Air conditioning units and other mechanical equipment should also be screened from public view using similar methods.



List of Preferred Plants

All plants listed are hardy in this region and have proven to do well in urban conditions.

Street Trees

Red Sunset Red Maple
 Halka Honey Locust
 Pin Oak*
 Village Green Zalkova
 Celebration Maple

Park/Public Open Space Trees

Sugar Maple*
 American Sweet Gum*
 Red Maple*
 Red Oak*
 American Beech*
 Tulip Tree*

Ornamental/Flowering Trees

Shadblow Serviceberry*
 Pink Flowering Dogwood
 White Flowering Dogwood*
 Milky Way Dogwood
 Sugar Tyme Crab
 Snowdrift Crab
 Redspire Pear

Evergreen Trees

Eastern White Pine*

Ground Covers

Royal Beauty Cotoneaster
 Purpleleaf Wintercreeper
 Tam's Juniper
 Blue Myrtle
 Pink Tree Wisteria

Evergreen Shrubs

Boudoir Azalea
 Delaware Valley Azalea
 Palestrina Azalea
 Boxwood
 Ivory Jade Euonymus
 Nordic Holly
 Blue Boy Holly
 Blue Girl Holly
 Japanese Andromeda
 Mugho Pine
 Album Elegan Rhododendron
 English Roseum Rhododendron
 Chionoidis Rhododendron
 Wilson Rhododendron
 PJM Rhododendron
 Hicks Yew

Deciduous Shrubs

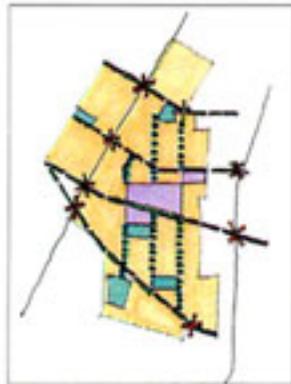
Crimson Pygmy Barberry
 Silverblotch Dogwood
 Kelsey's Dogwood
 Red Osier Dogwood
 Dwarf Burningbush
 Ann Magnolia
 Northern Bayberry*
 Maries Doublefile Viburnum

**Native plants*

Appendix B: Street Improvements



I Street Improvements



THE CHARACTER OF A NEIGHBORHOOD relies heavily on the character of its streets. Currently, many neighborhood streets in the JeffVanderLou community are in disrepair, with many missing sidewalks and curbs. Street landscaping, which can contribute a great deal to the overall atmosphere of the neighborhood, is also lacking.

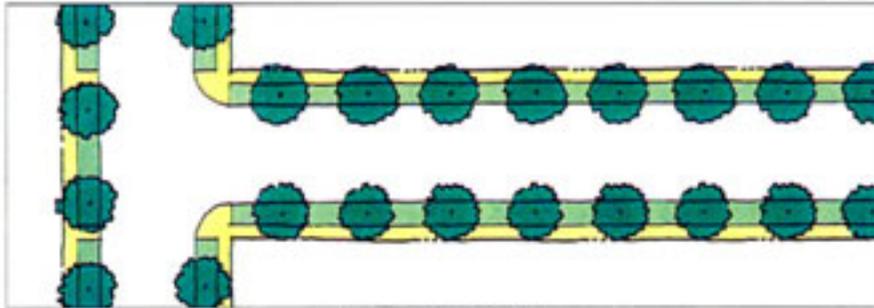
B-1

General streetscaping initiatives involve the improvement of sidewalks and landscaping throughout the neighborhood. Some special streets can also be created. North-south streets that connect important civic spaces, like Norman Seay Park and the new high school, especially Garrison Avenue, Webster Avenue and Glasgow Avenue, could be more heavily landscaped to create special park-like addresses. The major east-west connector streets, North Market Street, St. Louis Avenue, Cass Avenue, and Dr. Martin Luther King Jr. Drive, are wider roads which can receive landscaped medians.

On all streets, on-street parking should be clearly designated by landscaped bulb-outs at intersections, which serve to slow traffic as well as protect parked cars. Traffic-calming measures, such as mid-block and intersection roundabouts, play an important role in making neighborhood streets safer for children and pedestrians. Traffic-calming devices can also serve to make streets a more pleasant part of the public realm. A basketball court in the right-of-way, or a "hoops street," for example, can provide a neighborhood amenity and slow traffic.

Garrison Avenue as an example of a green street connecting neighborhood destinations such as schools and churches, with a bike path incorporated into the cartway.

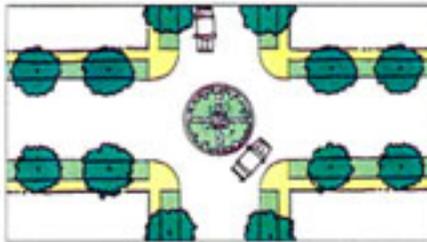




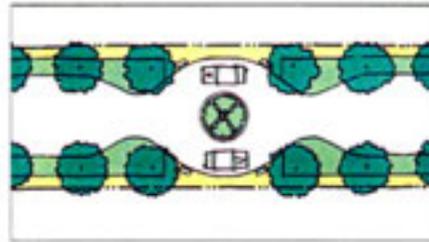
Typical Right of Way

Traffic-calming measures for neighborhood streets that make streets safer for the residents.

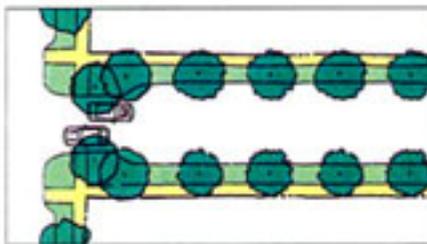
B-2



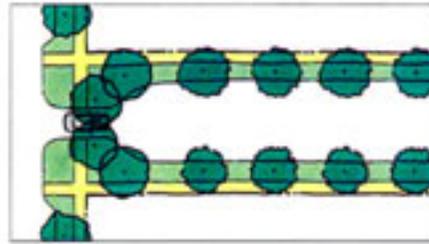
Roundabout at Intersection



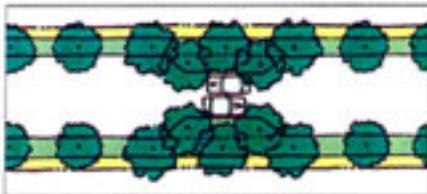
Mid-Block Roundabout



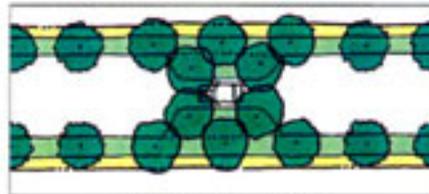
Bulb-Out at Intersection



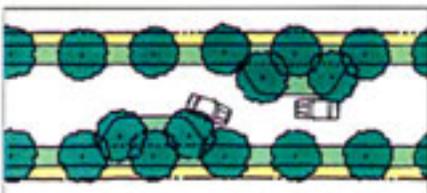
Bulb-Out at Intersection



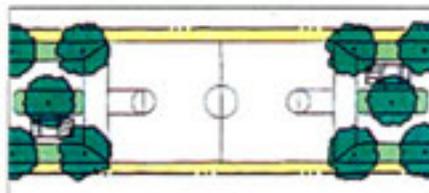
Mid-Block Bulb-Out



Mid-Block Bulb-Out



Chicane



Hoops Street



Dr. Martin Luther King Jr. Drive
Before (below) and after (left) street improvements.



Grand Boulevard
Before (below) and after (left) street improvements typical for a commercial thoroughfare.



Garrison Avenue
A linear park includes multi-use trails as well as bike paths.

Appendix C: Cost Estimates



Infrastructure and Landscaping Costs

1) The Wedge: Improvements would include a new park at the corner of Franklin, MLK, & Leffingwell. Also included would be improvements to the landscaping of streets immediately adjacent to the park.

Wedge Cost Analysis

Item 1	landscaping of park - grass, flowers, shrubs, paths (24,375 s.f. x \$3.00)	\$	73,125.00
Item 2	allowance for monument in park	\$	20,000.00
Item 3	landscaping of adjacent streets - grass, flowers, shrubs (700 l.f. x \$70)	\$	49,000.00
Item 4	18' tall period lighting fixtures in park & on adjacent streets (\$3500 per fixture installed, one fixture per 100')	\$	24,500.00
Item 5	street improvements (new curbs, sidewalks, pavement cold mill and asphalt overlay, 700 l.f. x \$260)	\$	182,000.00
Total:		\$	348,625.00

2) Webster Avenue Street Improvements: Improvements would include new curbs, pavement, sidewalks, landscaping, lighting, median and two small parks. Improvements extends from Magazine to Chambers Park

Webster Avenue Cost Analysis

Item 1	landscaping - grass, flowers, shrubs (2800 l.f. x \$35)	\$	98,000.00
Item 2	street improvements - portion of street without median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 1200 l.f. x \$260)	\$	312,000.00
Item 3	street improvements - portion of street with median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 1600 l.f. x \$317)	\$	507,200.00
Item 4	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	98,000.00
Item 5	landscaping at two small parks on MLK (75,500 l.f. x \$3)	\$	226,500.00
Total:		\$	1,241,700.00

3) Grand & Dr. Martin Luther King Jr. Intersection Improvements: Improvements would include new curbs, pavement, sidewalk, landscaping, lighting and medians. The improvement area extends from the intersection 300' in all directions.

Grand & MLK Cost Analysis

Item 1	street improvements - portion of street without median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 600 l.f. x \$260)	\$	156,000.00
Item 2	street improvements - portion of street with median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 600 l.f. x \$317)	\$	190,200.00
Item 3	bulb out traffic calming devices (\$25,000 per set x 2 sets)	\$	50,000.00
Item 4	landscaping along curbs - grass, flowers, shrubs (1200 l.f. x 70)	\$	84,000.00
Item 5	landscaping at median - grass, flowers, shrubs (600 l.f. x \$20)	\$	12,000.00
Item 6	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	42,000.00
Item 7	lighting at median - 18' period lighting fixtures (\$3500 per fixture installed, one fixture per 100')	\$	21,000.00
Total:		\$	555,200.00

4) Grand & St. Louis Ave Intersection Improvements: Improvements would include new curbs, pavement, sidewalks, landscaping, lighting and medians. Improvement area extends from intersection 300' in all four directions.

Grand & St. Louis Ave. Cost Analysis

(see grand & MLK cost analysis)			
Total:		\$	555,200.00

5) Industrial Edge: Costs compiled for 1000 l.f. of typical street.

Industrial Edge Cost Analysis

Item 1	street improvements (\$260 per l.f.)	\$	260,000.00
Item 2	landscaping - grass, flowers, shrubs (\$35 per l.f.)	\$	35,000.00
Item 3	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	35,000.00
Total:		\$	330,000.00

6) Dr. Martin Luther King Jr. Street Improvements: Improvements would include new curbs, pavement, sidewalks, landscaping, lighting and medians. The improvement area runs from the Wedge improvement area to the MLK/Grand improvement area.

MLK Street Improvements Cost Analysis

Item 1	street improvements - assumes entire street with median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 4700 l.f. x \$317)	\$	1,489,900.00
Item 2	landscape curb areas - grass, shrubs, flowers (4700 l.f. x \$70)	\$	329,000.00
Item 3	landscape median - grass, shrubs, flowers (4700 l.f. x \$35)	\$	164,500.00
Item 4	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	164,500.00
Item 5	25' tall lighting at median (\$5000 per fixture, one fixture per 100')	\$	235,000.00
Item 6	park 'A' (landscaping, lighting, site improvements for 28,000 s.f.)	\$	320,000.00
Item 7	park 'B' (landscaping, lighting, site improvements for 7500 s.f.)	\$	175,000.00
Total:		\$	2,877,900.00

7) Grand Avenue Street Improvements: The improvement area runs from Grand/St. Louis improvement Area to Grand/MLK improvement area. Improvements would include new curbs, pavement, sidewalk, landscaping, lighting and medians.

Grand Ave. Street Improvement Cost Analysis

Item 1	street improvements - assumes entire street with median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 3450 l.f. x \$317)	\$	1,093,650.00
Item 2	landscape curb areas - grass, shrubs, flowers (3450 l.f. x \$70)	\$	241,500.00
Item 3	paved median (3450 l.f. x \$50)	\$	172,500.00
Item 4	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	119,000.00
Item 5	lighting at median (\$5000 per 25' period lighting fixture, one fixture per 100')	\$	170,000.00
Total:		\$	1,796,650.00

8) St. Louis Avenue Street Improvements: The improvement area runs from Glasgow to Prairie. Improvements would include new curbs, pavement, sidewalk, landscaping, lighting and medians. Costs of improvements extending 300' in either direction from Grand Ave. are not included in this estimate. See the estimate for the Grand/St. Louis Ave intersection for this information.

St. Louis Ave. Street Improvement Cost Analysis

Item 1	street improvements - assumes entire street with median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 1750 l.f. x \$317)	\$	554,750.00
Item 2	street improvements - portion of street without median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 300 l.f. x \$260)	\$	78,000.00
Item 3	landscape curb areas - grass, shrubs, flowers (2050 l.f. x \$70)	\$	143,500.00
Item 4	landscape median - grass, shrubs, flowers (1750 l.f. x \$35)	\$	61,250.00
Item 5	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	70,000.00
Total:		\$	907,500.00

9) Cass Avenue Street Improvements: The improvement area runs from Spring to Elliot. Improvements would include new curbs, pavement, sidewalk, landscaping, lighting, and a small park.

Cass Ave. Street Improvement Cost Analysis

Item 1	street improvements - portion of street without median (new curbs, sidewalks, pavement cold mill and asphalt overlay, 5100 l.f. x \$260)	\$	1,326,000.00
Item 2	landscape curb areas - grass, shrubs, flowers (5100 l.f. x \$70)	\$	357,000.00
Item 3	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	178,500.00
Item 4	park - trees, grass, shrubs, flowers, paths (75,000 s.f. x \$3.00 per s.f.)	\$	225,000.00
Item 5	street improvements around park (950 l.f. x \$260)	\$	247,000.00
Item 6	landscaping adjacent to park (950 l.f. x \$35)	\$	33,250.00
Item 7	lighting at sidewalk adjacent to park (\$3500 per 18' period lighting fixture, one fixture per 100')	\$	31,500.00
Total:		\$	2,398,250.00

10a) Garrison Avenue - Volkswagen Scheme (for 1000 l.f.): improvements would include new curbs, pavement, sidewalks, landscaping and lighting. The basic improvements would work with the existing street configuration. The improvement area runs from St. Louis Ave. to Magazine.

Garrison Ave. - Volkswagen Scheme Cost Analysis

Item 1	street improvements (new curbs, sidewalks, pavement cold mill and asphalt overlay, 2500 l.f. x \$260)	\$	650,000.00
Item 2	landscaping curb areas (2500 l.f. x 35)	\$	87,500.00
Item 3	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	87,500.00
Total:		\$	825,000.00

10b) Garrison Avenue - Volkswagen Scheme (for 1000 l.f.): improvements would include new curbs, pavement sidewalk, landscaping and lighting. These enhanced improvements would introduce gently curving streets small parks and generous landscaping to the existing street configuration. The improvement area runs from St. Louis Ave. to Magazine.

Garrison Ave. - Cadillac Scheme Cost Analysis

Item 1	street improvements (new curbs, sidewalks, pavement cold mill and asphalt overlay, 2500 l.f. x \$260)	\$	650,000.00
Item 2	landscaping curb areas - grass, shrubs, flowers, trees (2500 l.f. x \$80)	\$	200,000.00
Item 3	additional landscaping for small parks	\$	72,000.00
Item 4	18' tall period lighting fixtures at sidewalks (\$3500 per fixture installed, one fixture per 100')	\$	87,500.00
Total:		\$	1,009,500.00

11a) Traffic Calming Devices - Bulb-outs: These would be introduced into residential areas to slow traffic.

Bulb-out Cost Analysis

Item 1	bulb out traffic calming devices (\$25,000 per set)	\$	25,000.00
Item 2	landscaping curb areas - grass, shrubs, flowers	\$	5,000.00
Total:		\$	30,000.00

11b) Traffic Calming Devices - Basketball Street (Hoop Street) These would be introduced into residential areas to slow traffic.

Hoops Street Cost Analysis

Item 1	street improvements - bulb-outs, stripes, goal posts	\$	35,000.00
Item 2	landscaping curb areas - grass, shrubs, flowers	\$	5,000.00
Total:		\$	40,000.00

Clarification: This is a summary of cost information pertaining to specific initiatives outlined in the JeffVanderLou Draft Plan from the May 2000 charette in St. Louis. The estimates have been compiled from information provided by the Civil Engineering Department at David Mason & Associates, the Landscape Architects for the project, LaQuatra Bonci Associates, and the City of St. Louis Board of Public Service.



Master Plan

Blue	Institutional
Green	Park
Yellow	New Residential
Light Green	Existing Residential
Red	Commercial
Orange	Mixed Use
Brown	Industrial

Vashon/JeffVanderLou Initiative
 St. Louis, Missouri

Tab B

Previous Planning Efforts

Previous planning efforts within the JeffVanderLou neighborhood and in neighborhoods immediately adjacent have had both positive and negative impacts on the area over time. Perhaps the first planning decision of note was the decision by the St. Louis Board of Education to build a new high school on the site of what is now Harris-Stowe State College. This new school, named for the prominent African-American educator John Vashon, served the large African-American population living in segregated neighborhoods bordering the Mill Creek Valley – at the time an overcrowded area with extremely poor quality housing and living conditions. The quality of the education at Vashon became a source of pride among students and alumni in the decades to follow.

The next milestone was the razing of the Mill Creek Valley community in the late 1950s. Nearly 5,000 families were relocated in the first major use of the Missouri Urban Redevelopment Corporation statute (Chapter 353) and the Municipal Land Clearance for Redevelopment Law (Chapter 99). At the time the project earned the neighborhood a new name, "Hiroshima Flats," for the rapid clearance of homes, businesses, and churches.

With housing segregation a matter of fact in St. Louis at the time, many relocated families had nowhere to go. Public housing, most notably the Pruitt-Igoe housing development, became the only choice for many families. Others moved to the African-American neighborhoods north of Delmar Boulevard. The demolition of Pruitt-Igoe in 1976, then acknowledged as a disaster, forced hundreds more families to seek housing alternatives in JeffVanderLou and other near north neighborhoods.

As a result of the Mill Creek project and the dispersal of residents throughout the north side, Vashon High School enrollment dropped. The School Board closed the facility in 1963, turning it over to Harris Teacher College, and moved Vashon High School to a former vocational school at Bell and Grand Avenues.

In the 1960s and 1970s, residents began the first truly comprehensive attempt at revitalizing the neighborhood, led by Macler Shepard and Leon Strauss. With broad community support, Shepard and his partners rehabilitated hundreds of housing units, built elderly housing facilities, and provided day care, social services, and business development assistance. During this time the neighborhood participated in the Model Cities program. Over time, the effects of a depressed economy, the concentration of low income renters, out migration of the middle class, and transfer of ownership of rental property to absentee landlords contributed to the decline of the JeffVanderLou neighborhood.

Today's planning effort again looks to Vashon High School as the anchor of a proud community. The Vashon/JeffVanderLou Initiative seeks to build on the promise of excellence in education to encourage a renewed vitality in the neighborhood.

Thanks to John P. Mahoney, Ph.D. for material contained in his excellent history of Vashon High School and the JeffVanderLou neighborhood entitled *The History of Vashon High School*.

Tab C

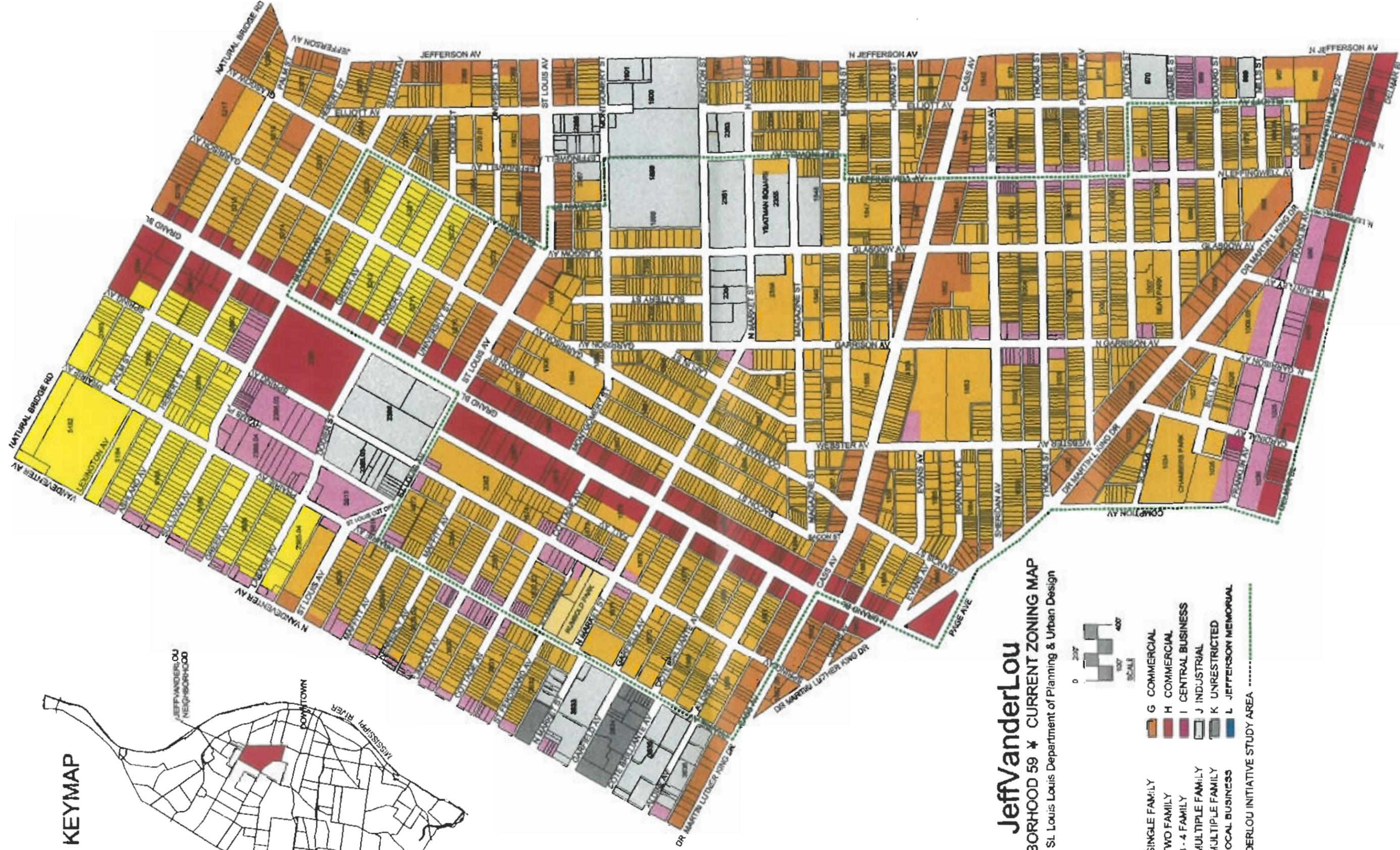
Existing Conditions: Zoning Summary

Residential Zoning: The existing zoning for this area is dominated by the 'C' Multiple Family zoning category. All of the residential parcels in the JeffVanderLou Initiative Study Area are zoned in this manner, except for a small area north of St. Louis Ave. The decline of the neighborhood has been partially attributed to absentee landlord neglect of property in the neighborhood. Changing the zoning in the residential areas to 'A' Single Family or 'B' Two Family may help encourage home ownership and discourage abuse of future housing in the neighborhood by absentee landlords.

Commercial Zoning: Many of the parcels along Leffingwell Ave. and North Garrison Ave. south of Cass Ave. are zoned 'F' Neighborhood Commercial. Parcels west of Grand Blvd. on Prairie Ave. and Spring Ave. are also zoned in this manner. On St. Louis Ave, N. Jefferson Ave and Cass Ave. many parcels are currently zoned 'G' Local Commercial. A recent driving tour of these streets revealed very few operating businesses on these parcels and many vacant buildings and lots. It might be advantageous to change the zoning in these areas to a residential classification. This might help solidify development plans in the residential areas, as developers would not be concerned about undesirable business locating on these parcels. This also might encourage new commercial development in the area to locate on larger, busier streets such as Grand or Martin Luther King. In general, it might be advantageous to reconsider all commercial zoning in the area to clarify if down zoning to more restrictive commercial zoning classifications might contribute to the well being of the area.

Industrial Zoning: 'J' Industrial Zoning is concentrated mostly in the northeast quadrant of the study area around Warner Jenkinson. It might be advisable to encourage any future similar uses to be developed in this area. This would allow the remainder of the area to develop without concern of industrial development in residential and commercial zones.

KEYMAP



JeffVanderLou NEIGHBORHOOD 59 ✦ CURRENT ZONING MAP

City of St. Louis Louis Department of Planning & Urban Design



- A SINGLE FAMILY
- B TWO FAMILY
- C 3 - 4 FAMILY
- D MULTIPLE FAMILY
- E MULTIPLE FAMILY
- F LOCAL BUSINESS
- G COMMERCIAL
- H COMMERCIAL
- I CENTRAL BUSINESS
- J INDUSTRIAL
- K UNRESTRICTED
- L JEFFERSON MEMORIAL

JEFFVANDERLOU INITIATIVE STUDY AREA-.....

Tab D

Demographics: JeffVanderLou Initiative Area

Population

	1990	1998 (est.)	% Change
JeffVanderLou	8,177	6,538	-20.0%
City of St. Louis	396,685	337,350	-15.0%

Households

	1990	1998 (est.)	% Change
JeffVanderLou	2,903	2,327	-19.84%

Demographics - JeffVanderLou-1998 Est.

Race	Number	% of Total Population
African-American	6,506	99.2%
White	28	0.5%
Other	22	0.3%
Age	Number	
Under 6	791	12.1%
6-17	1,395	21.3%
18-24	688	10.5%
25-34	901	13.8%
35-44	750	11.5%
45-54	556	8.5%
55-64	465	7.1%
65 and over	992	15.2%
Median age	29.9	

Income- JeffVanderLou-1998 Est.

	# Households	% of Total Households
Less than \$15,000	1,359	58.4%
\$15,000-24,999	353	15.2%
\$25,000-34,999	259	11.1%
\$35,000 or more	356	15.3%
1998 Median Income (est.)	JVL	City of St. Louis
	\$13,672	\$19,458

Education- JeffVanderLou-1998 Est.

Highest Level Achieved	% of Population Over 25
Elementary	25.0%
Some High School	29.8%
High School Diploma	40.9%
Undergraduate Degree	2.9%
Graduate/Professional Degree	1.2%

Housing Tenure

Status	Number	Percent
Owner-occupied	837	36.0%
Renter-occupied	1,490	64.0%

Source: AREA, Inc. Housing Market Assessment
Data obtained from Claritas

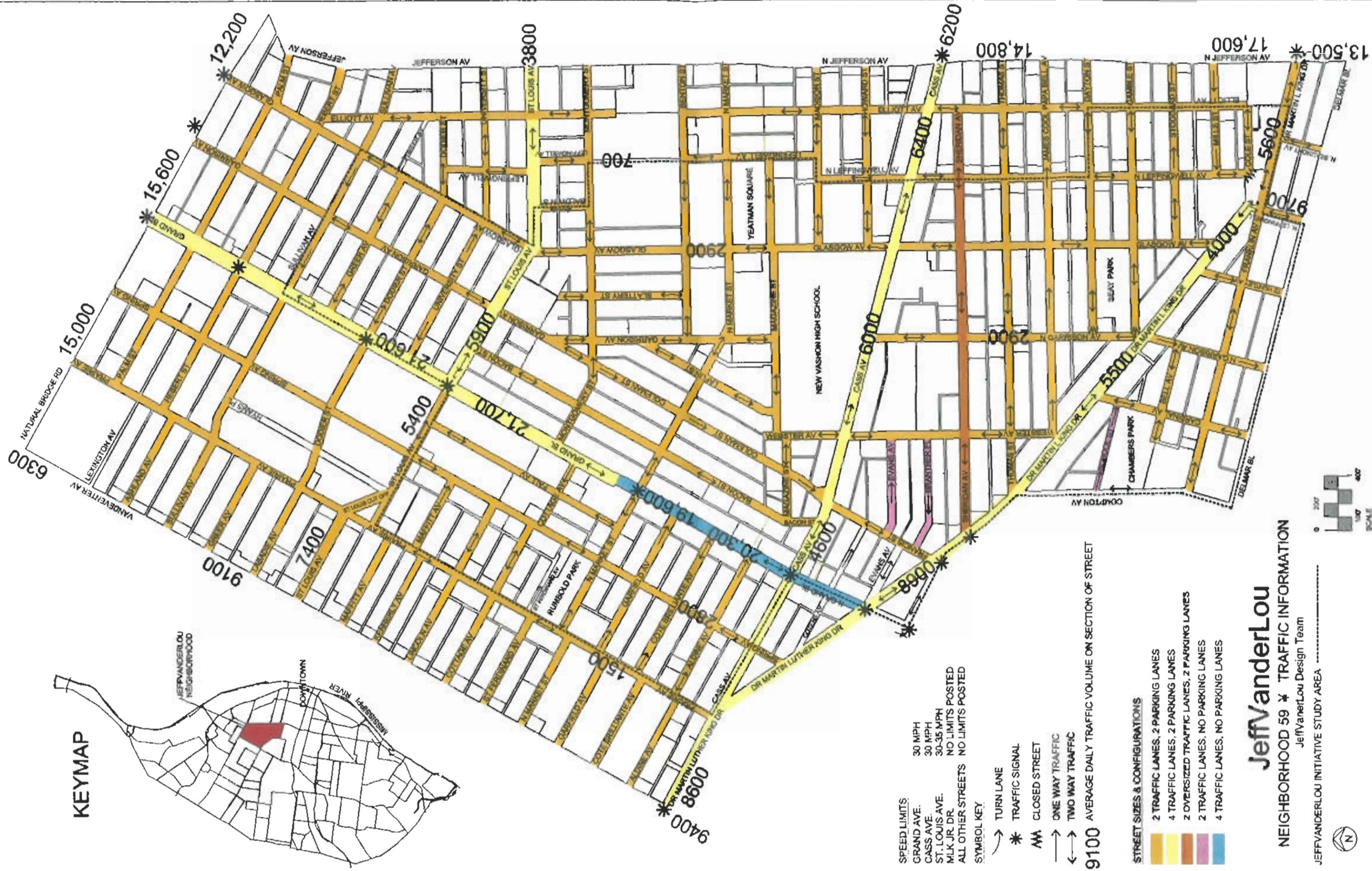
Tab E

Transportation and Circulation

Streets: The existing street grid is more than adequate to handle the development planned for the area. It was originally designed to handle a population far greater than what now exists in the area. Most of the streets are designed to allow on-street parking on both sides. Few one way streets or closed streets exist currently.

Public Transportation: Several bus routes run through the neighborhood, giving residents direct access to all parts of the metro area.

KEYMAP



SPEED LIMITS
 GRAND AVE. 30 MPH
 CASS AVE. 30 MPH
 ST. LOUIS AVE. 30-35 MPH
 MLK JR. DR. NO LIMITS POSTED
 ALL OTHER STREETS NO LIMITS POSTED

SYMBOL KEY

- TURN LANE
- * TRAFFIC SIGNAL
- AA CLOSED STREET
- ONE WAY TRAFFIC
- ←→ TWO WAY TRAFFIC

9100 AVERAGE DAILY TRAFFIC VOLUME ON SECTION OF STREET

- STREET SIZES & CONFIGURATIONS**
- 2 TRAFFIC LANES, 2 PARKING LANES
 - 4 TRAFFIC LANES, 2 PARKING LANES
 - 2 OVERSIZED TRAFFIC LANES, 2 PARKING LANES
 - 2 TRAFFIC LANES, NO PARKING LANES
 - 4 TRAFFIC LANES, NO PARKING LANES

JeffVanderLou

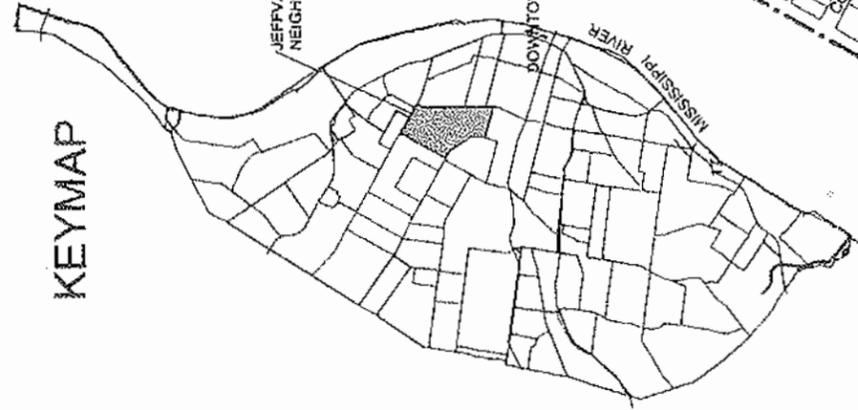
NEIGHBORHOOD 59 ✦ TRAFFIC INFORMATION

JeffVanderLou Design Team

JEFFVANDERLOU INITIATIVE STUDY AREA



KEYMAP



NEIGHBORHOOD BUS ROUTES

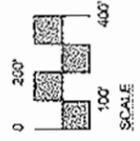
- 1 VANDEVENTER
- 4 NATURAL BRIDGE
- 15 HOODAMONT
- 19 ST. LOUIS AVENUE
- 30 CASS
- 32 WELLSTON-MARTIN LUTHER KING JR.
- 70 GRAND

JeffVanderLou

NEIGHBORHOOD 59 ✦ CURRENT BUS ROUTES

JeffVanderLou Design Team

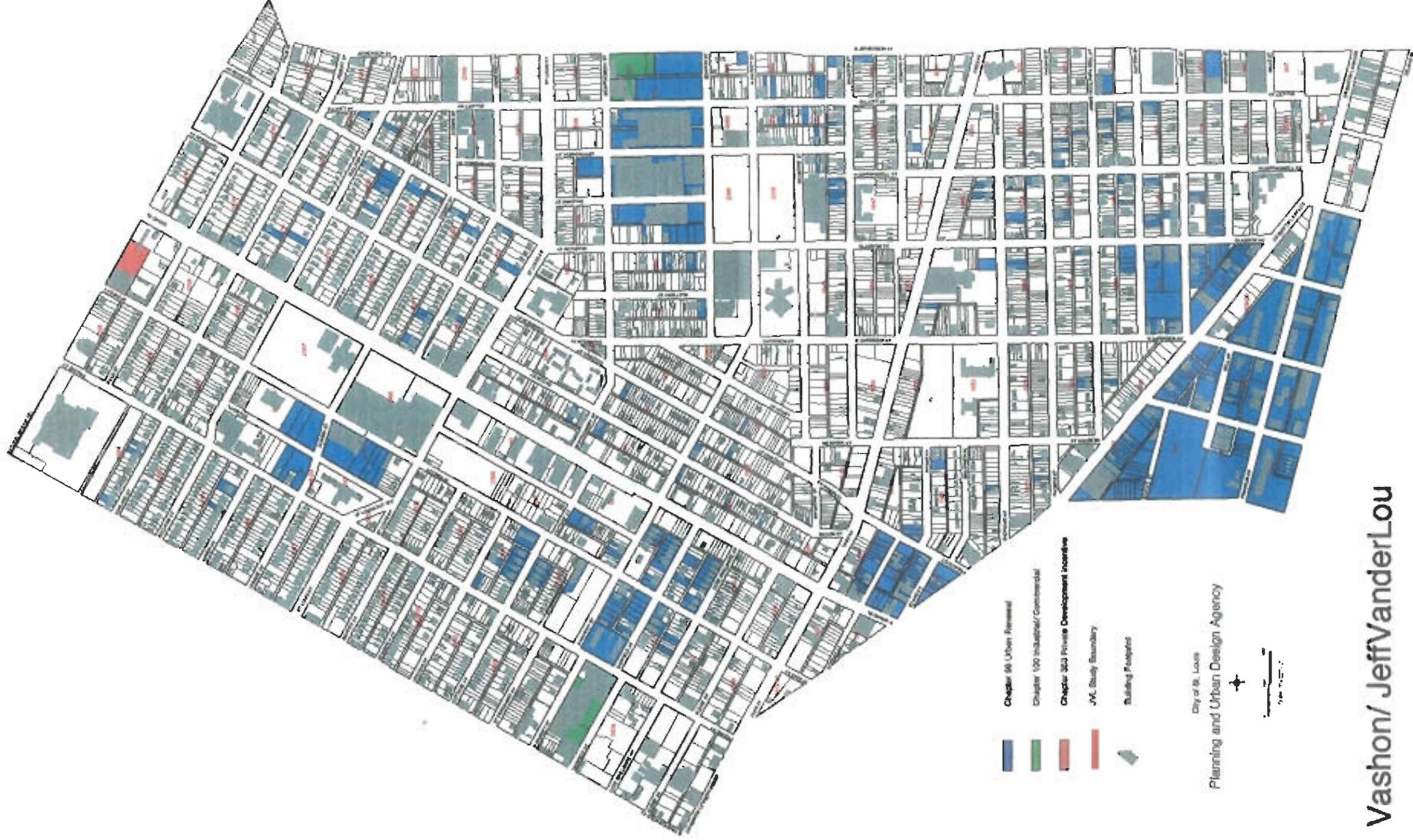
JEFFVANDERLOU INITIATIVE STUDY AREA



Tab F

Known Operating Redevelopment Plans

The St. Louis Development Corporation (SLDC) provided information regarding redevelopment plans (99s, 100s, and 353s) in effect in the JeffVanderLou neighborhood. These are summarized on the following table, and shown on the map on the following page. In some cases information was not available regarding the current status of tax abatement for subject addresses. This section relies solely on the information provided by the SLDC and makes no representation regarding the accuracy or completeness of the information.



- Chapter 96 Urban Renewal
- Chapter 100 Industrial/Commercial
- Chapter 203 House Development Incentive
- JVC Study Boundary
- Building Footprint

City of St. Louis
 Planning and Urban Design Agency

Vashon/ JeffVanderLou



Tab F: Known Operating Redevelopment Plans

Name	Address	Type	Area (Acres)	Ordinance Number	Approval Date	Permitted Uses	Tax Abatement End Date	Eminent Domain
Lindell's Addition	3616, 3624 Natural Bridge 3601 N. Grand 3637 Palm Ave.	353		59834	March 19, 1986		December 31, 2007	
Commercial/Industrial	3813 Garfield	100	3.28	60400	July 24, 1987	Commercial/Industrial		
Lindell's Addition	3728 Palm	353		60316	March 4, 1987			yes
Stoddard Addition	2600-12 Stoddard	353		60656	February 17, 1988			yes
Jackson's Subdivision	3718-20 Cottage Ave.	353		61075	November 17, 1988		December 31, 2007	yes
2950 N. Market Ave.	2950 N. Market Ave.	353	3.12	61065	November 17, 1988			yes
Dayton/Dr. Martin Luther King/N. Garrison/Glasgow	several	99	12.08	63624	January 23, 1996	various		yes
2933-75 Dr. Martin Luther King Jr. Drive	2933-75 Dr. Martin Luther King Jr. Drive	99	1.40	62915	March 2, 1994	Commercial/Institutional		yes
Garfield Ave. Scattered Sites	3616-24 N. Market St. 3615-21 Garfield Ave. 3627 Garfield Ave. 3635-37 Garfield Ave. 3645-51 Garfield Ave.	99		63754	April 8, 1996	Residential		no
Sullivan Ave.	3203 & 3225 Sullivan Ave.	99		63240	July 25, 1994	Residential		yes
North Grand Blvd.	3608 N. Grand Blvd.	99	0.20	63623	January 23, 1996	Commercial		no
Hebert St.	3500 & 3521 Hebert St.	99		64245	January 21, 1998	Residential		yes
Greer Ave.	3815 Greer Ave.	99	0.25	64137	August 1, 1997	Residential		yes
North Spring/Dodier	2901-11, 2900-18 Hyams Place 2901-17 N. Spring Ave. 3701-51 Dodier Ave.	99	2.20	64249	January 21, 1998	Residential/Institutional		yes
Grand Oak Village	several	99	1.40	64535	January 4, 1999	Residential		no
St. Louis Ave./N. Spring Ave.	several	99	3.00	64827	December 21, 1999	Commercial/Institutional		yes
N. Jefferson	2401 N. Jefferson Ave. 2609-11 N. Market Pl. 2513 N. Market Pl.	99	0.13	64835	December 29, 1999	Residential		no
Ashland/Greer	3821, 3827, 3863 Ashland Ave. 3833 Greer Ave.	99	0.52	65008	August 1, 2000	Residential		yes
Prairie/Lincoln/Spring	2614-16 Prairie 3721-31 & 3726-40 Lincoln Ave. 2604-14 N. Spring Ave.	99		65054	October 5, 2000	Residential		no
JVL Renaissance I Area	57 sites	99	6.80	64967	July 10, 2000	Residential		limited
Grand Oak Village	32 sites	99	4.00	64158	October 14, 1997	Residential		no

Tab G

Known Proposed Expansions by Business or Industry

Based on information provided by staff of the St. Louis Development Corporation (SLDC), there is one known proposed expansion by industry within the JeffVanderLou planning area: the Warner-Jenkinson company plans to expand roughly south to the north side of Yeatman Square, west to the eastern side of the alley behind Glasgow, north to St. Louis Avenue, and east to Jefferson Avenue. Warner-Jenkinson officials were involved in the neighborhood planning process and are interested in continuing a dialogue with residents and the Initiative regarding expansion plans and buffer zones between industry and residential areas. The Master Plan seeks to permit industrial expansion within existing industrial areas, while maintaining an adequate and effective buffer zone between uses.

Tab H

Infrastructure Conditions and Capacity

Streets: Our traffic consultant inspected the streets and noted their relatively good condition considering their age. The paved surfaces are in acceptable shape (with some need for improvement in certain areas) and the granite curbs are in great shape.

Alleys: The alleys generally are overgrown with weeds and volunteer trees. The brick pavers in the alleys are generally in good shape, but many have been uprooted.

Water: Several large mains transverse the neighborhood. The Water Division recommends that one of these be replaced before any large-scale development occurs in its vicinity. The neighborhood is divided into two pressure zones north and south of Cass. South of Cass, the water pressure can be as much as 40 psi less than the pressure north of Cass. This could be remedied by relocating the pressure point in this area. In general the Water Division representative stated that service to new individual residences should not be an issue.

Electric: The Ameren Union Electric representative stated that the area is prepared for the development we have planned. He further stated that the City might prefer that any new service to individual residences be located underground. Ameren UE is prepared to provide conventional or underground service.

Telephones: Southwestern Bell's representative stated that the phone service in this area is adequate to handle the development we have proposed. She further stated that should development occur, existing conventional lines would be converted to fiber optic.

Lighting: The present state of street lighting in JVL is average. As new development occurs, it would be important to improve street lighting from a safety standpoint.

Sewers: The Metropolitan Sewer District representative stated that the sewers in the area are prepared for future residential and light commercial development.

Gas: We were unable to reach a Laclede Gas representative in time for the publication of this document.

Tab I

Capital Improvement Program

Capital improvements to Dunbar and Yeatman Parks are underway with \$800,000 in funding allocated by Alderman Michael McMillan. The improvements include:

Yeatman Park

Demolish existing playground, comfort station and basketball court

Regrade

Install full-size basketball court

Install five tennis courts

Install lighting for basketball and tennis courts

Perimeter walk and curb work

Dunbar Park

Demolish existing playground

Regrade

Acquire property

Grade for baseball field (minimum minor league size)

Parking lot – minimum 90 spaces

Perimeter walk and curb work

Tab J

Demographic/Economic Impacts

The JeffVanderLou Initiative is an all-encompassing effort – covering housing, economic development, health and human services, and education. The process of revitalization is expected to generate new economic activity and encourage growth in the neighborhood population by attracting new residents to the neighborhood. Given the many challenges – and opportunities – facing the neighborhood, change will take place over a period of several years.

In an attempt to quantify potential impacts of housing and commercial development, two market studies were done by Applied Real Estate Analysis, Inc. (AREA) of Chicago. AREA is a real estate and planning policy consulting firm. These market studies are entitled *JeffVanderLou Housing Market Assessment* and *JeffVanderLou Retail Market Assessment*. Their conclusions are summarized briefly below.

Housing Market Assessment

This report examined area demographics and socio-economic conditions, and analyzed the market for for-sale, rental, and elderly housing. Based on *existing neighborhood conditions*, AREA estimated that a total of 113 new housing units could be developed in year one of the housing development effort. AREA's recommended unit mix included 18 for-sale single-family or townhouse units, 25 rental units, and 70 elderly rental housing units. *It must be noted that the JeffVanderLou resident priorities and action plan emphasize single-family homeownership, not rental development.* Existing projects in JeffVanderLou supply substantial numbers of rental units, including newly-renovated affordable units. The AREA report suggested future annual development of 18 single-family and 25 rental units per year.

These estimates are very conservative. It is extremely difficult to predict the effect the overall neighborhood revitalization in JeffVanderLou will have on housing demand. However, the Initiative expects that, with resources concentrated on development for homeownership, and with financial incentives to recruit the first homebuyers, the JeffVanderLou neighborhood could support much more than 18 new housing units per year. Development estimates will be refined as specific development opportunities are identified. Development will focus on homeownership, as indicated by the subcommittees during the planning process.

Retail Market Assessment

This report evaluated market characteristics, identified trade areas, and looked at existing and potential retail corridors in JeffVanderLou. Following are key excerpts from the Executive Summary.

***Demand for Convenience and Comparison Retail Goods.** An analysis of retail expenditure potential and capture revealed that there is unmet demand for various types of convenience and comparison goods and related services in the trade areas. Some of this unmet demand could be addressed by additional retail facilities in the

JeffVanderLou neighborhood. Convenience goods are retail items that consumers generally are not willing to travel long distances in order to purchase, while comparison goods are retail items that consumers are willing to comparison shop before purchasing. Within the category of convenience goods we estimate there is sufficient demand support for food-at-home, food-away-from-home, and personal-care retail purchases in the short term. There is also unmet demand for the following comparison retail purchases: apparel and accessories, furnishings and supplies, general merchandise, and miscellaneous items which include books and other reading materials, entertainment, stationery and cards, flowers, jewelry, etc. However, while this demand exists, we would only recommend developing comparison retail in the short term if these uses are part of a grocery-anchored community shopping mall. Comparison retail uses are likely to experience greater sales per square foot if incorporated into a shopping mall rather than as stand alone businesses.

"Expenditure Potential and Capture. Given the lack of diverse retail options in JeffVanderLou, a high percentage of retail expenditures are leading out of the community. Currently, there is sufficient unmet demand in the combined trade areas to capture more than \$25 million in convenience retail expenditures. Food-at-home purchases, at over \$13 million, would represent the greatest share of convenience expenditures. In the category of comparison goods and services more than \$11 million could be captured if vendors were part of a grocery-anchored neighborhood shopping mall. In this category, miscellaneous retail purchases such as reading materials, stationery, barber shops, beauty salons, dry cleaners, card stores, florist, etc. would capture the greatest share of expenditures at approximately \$4 million. Overall, there is unmet demand to support more than 85,000 square feet of convenience-related retail space and more than 58,000 square feet of comparison-related retail space. As JeffVanderLou is revitalized over time, the area's retail expenditure potential will increase."

Additional Anticipated Economic Impacts

Historic and Cultural District

One of the primary resident priorities for JeffVanderLou, detailed in the Action Plan, is the celebration of the history of the neighborhood. An effort is already underway to research and document the vibrant history of JeffVanderLou and to begin the process of achieving recognition at the state and federal level as an historic neighborhood. By building on the entertainment, academic, and athletic history of JeffVanderLou, we can build a base for tourism and commercial activity along Dr. Martin Luther King Jr. Drive. Many prominent African-American cultural and historic sites are located in and adjacent to the neighborhood, and can be used to build an historic and cultural district centered in JeffVanderLou.

Employment and Entrepreneurship

Other development activities detailed in the JeffVanderLou action plan should produce positive economic activity. Housing and commercial development will open the door for employment for residents. The Initiative is planning to bring effective training and job preparation programs to the neighborhood to prepare residents to take advantage of these opportunities.

The Action Plan includes several employment strategies geared to advance JeffVanderLou residents into higher paying jobs in technology fields. Other efforts will work with area employers so that residents are trained in job sectors that are seeking workers.

Entrepreneurial opportunities will be available for residents. The Initiative has already begun forming partnerships with major St. Louis institutions such as the Missouri Botanical Garden, and with other partners, that will train residents to own and operate viable businesses.

Tab K

Recommended Zoning

Introduction

Our rezoning recommendations are based upon the JeffVanderLou Master Plan published in October of 2000. The JVL Design Team Strongly recommends further consultation with the residents of JeffVanderLou before rezoning is finalized.

Option 1 Conventional Zoning

Our rezoning recommendations in Option 1 are based upon the residents' priorities and belief that a more stable community could be realized if more owner-occupied housing and small scale, mixed-use commercial developments existed. In this first rezoning option, we have worked within the bounds of the existing zoning code to suggest zoning that would help to achieve these results.

Residential Uses:

We propose a shift from 'C' Multifamily to 'A' Single Family and 'B' Two Family in the neighborhood. Owner occupied housing would be encouraged. Existing multiple-family housing would remain as nonconforming uses. Desirable multi-person developments such as elderly care facilities could be allowed through a change in zoning for the parcels in question. New multi-family development would be discouraged under this plan. Elderly and handicapped units could be built within either zoning district. The building codes would determine how these buildings would be specially designed for such uses.

Commercial Uses:

We have recommended that the uses on Martin Luther King between Jefferson and Grand be switched from 'G' Local Commercial to 'F' Neighborhood Commercial. This was done to encourage smaller scale mixed use development and to discourage strip mall development.

On Grand Ave between Martin Luther King and Sullivan, we have also recommended a change to the 'F' Neighborhood Commercial for the same reasons. This area is presently zoned 'H' Area Commercial

In the residential areas we have recommended eliminating nearly all of the 'F' Neighborhood Commercial zoning. Most of this is concentrated on Leffingwell and Garrison. Very few businesses remain in these areas. We believe that this will encourage future commercial developers to locate their projects on Grand or Martin Luther King.

Zoning Suggestions Outside our Boundaries

Rezoning recommendations suggested outside the JeffVanderLou Initiative Study Area are suggested with the goal of improving the entire area. We are aware that the scope of our planning work is only within the JVL Initiative boundaries.

Blumeyer Strip: We recommend that the portion of the Blumeyer Housing Development along Martin Luther King be rezoned to anticipate future development. We propose 'F'

Neighborhood Commercial to reflect our adjacent rezoning recommendations on Dr. Martin Luther King Jr. Drive.

Jefferson Avenue Strip: Between the JVL planning boundary and Jefferson Avenue, from Martin Luther King to Sullivan lies a narrow parcel of land that is located in the 5th Ward. We believe that it makes sense for us to consider the direction of this area because of its close proximity to the center of JVL and the high school and its isolation from the center of the 5th Ward. In considering this area, we studied the land use maps provided by the 5th Ward Planning Team. Our suggestions, though not identical to their map, contain the essence of the 5th Ward Team's idea for this area. Generally, our goal was to maintain as much of the residential zoning in this area as possible. We also thought it important to consider the future expansion of Warner Jenkinson and other industrial neighbors in the northeast corner of this area.

Carter Carburetor: Our intention in this area was to encourage the development of what is now an empty lot at the corner of St. Louis Avenue and Grand. We feel the best use for this lot would be 'F' Local Business. This would match the zoning we are proposing along most of Grand.

Martin Luther King Strip: Directly across the street from our planning area on Dr. Martin Luther King Jr. Drive near Jefferson lies a small commercial strip. We recommend this area be zoned the same as the neighboring commercial structures within our planning area. In this manner, development of both sides of Dr. Martin Luther King Jr. Drive would proceed evenly.

Rezoning Option 2

Traditional Neighborhood Development Codes

Traditional Neighborhood Development (TND) Codes represent a relatively new approach to zoning. Conventional zoning codes are either greatly adjusted or replaced in TND districts in favor of revised or TND codes. These new codes promote "human scale" mixed use, pedestrian oriented development. These codes are supported by a planning movement known as New Urbanism.

The New Urbanism:

The New Urbanism is a movement, founded in the late 1980s by a group of architects who were troubled by suburban sprawl and who wanted to find a way to promote the creation of more livable and people-oriented places. The movement has grown from a few dozen architects into a full-fledged Congress of several thousand architects, designers, builders, developers, city officials, environmentalists, and transportation experts.

The movement is guided by a set of principles known as the Charter. The principles move from the Regional Scale, to the local scale (cities, towns and villages), to the neighborhood scale (streets and blocks), to the building scale (down to comments on building style and the importance of local precedents).

The heart of the New Urbanism is at the neighborhood scale. The idea is to build neighborhoods that are compact, pedestrian-oriented and contain a mix of uses. This would allow people to walk to places of work, play, worship, and to public transportation. New Urbanism encourages a mix of incomes and a variety of housing styles (based on local precedents). New Urbanism suggests that neighborhoods should also be safe. This is

accomplished by having buildings face streets and parks. Front porches also help to draw people into the public realm - thereby also making the neighborhood safer.

New Urbanists often find that it is very difficult to build these kinds of neighborhoods because local zoning codes, and development ordinances and state and federal regulations (such as those promoted by Departments of Transportation) were established during the 1950's and 60's and are designed to build suburban, auto-oriented places.

The Problem with Traditional Zoning:

The problem with traditional, Euclidean zoning is that it is based on nuisance law. The original zoning was established at a time when cities were overcrowded, dirty, and often disease ridden. Zoning, or separating different land uses into different parts of the city, made sense under these conditions. It was good to make sure that noxious uses, such as slaughter houses, or glue factories, were kept away from areas where people lived or worked - both for health reasons, but also to protect property values.

The problem with this philosophy is that ever since the first zoning came into effect in New York around the turn of the last Century, planners have spent all of their effort trying to separate uses (while at the same time dividing uses into ever more narrow categories). Typical suburban zoning has everything spread apart. Offices go in one place. Shopping goes in another. Schools go in another location, and children are bused regardless of whether they live across the street or 20 miles away. Housing has been divided not only into single and multifamily districts but often by designating lot sizes into income categories as well. The entire city thus depends on the private automobile and huge public investments in roads to function.

The problem is just as bad when codes that were created to build suburbs are applied in older urban neighborhoods. Great urban neighborhoods rely on a mix of uses all located in close proximity. It makes sense to separate some uses - such as large scale commercial or heavy industry to locations that will not disturb residential areas. Most other uses, however, not only can be mixed together, but to create a successful neighborhood, should be mixed together. Houses of all kinds with owners of varied income levels would therefore face each other across the streets. Changes take place along the back property lines or alleys. Schools, churches, neighborhood shopping, parks, transit, etc. should all be mixed in close proximity.

Traditional Neighborhood Development Codes

In the past five or six years, a number of communities across the country have been developing New Urbanist Codes. For example: Austin, TX; Suffolk, VA; Milwaukee, WI; West Palm Beach, FL; and Columbus, OH (still being drafted).

Some, like Suffolk are pretty traditional with some provisions for Traditional Neighborhood Developments. Others, like Austin, are based in the traditional codes, but tend to favor New Urbanist developments. Most of these codes are illustrated so that it is clear what type of development is being promoted.

The TND zones typically allow a mix of uses, on smaller than typical lots (by suburban standards). They also encourage "main street" and corner neighborhood commercial development, shared parking, on-street parking counted toward parking counts, and parking

in the rear for commercial and office uses. They also try to encourage typical main street commercial buildings in terms of set backs, massing, sun shadow, and wall openings.

In terms of residential uses they stress a mix of housing types (again, like types face each other across the street, but are not separated into different areas), they encourage small blocks with alleys, lots of parks, narrow alleys and streets, on-street parking, interconnected streets (limiting cul-de-sacs) neighborhood parks surrounded by streets and faced with houses, neighborhood schools that children can walk to, corner stores, or shopping streets within walking distance from most residences. Setbacks are set to match typical pre-WWII neighborhoods in the area. Garages are accessed from the alleys, or if no alleys, garages must be set back from the front plane of the house so that they do not dominate the front facade. These codes also often try to influence street width and design.

Conclusion

In our second rezoning option, we have suggested a Traditional Neighborhood Development Code. Of the two options, this is the one the design team favors. Should the neighborhood wish to explore this option, the design team could create a specific TND Code for this area.

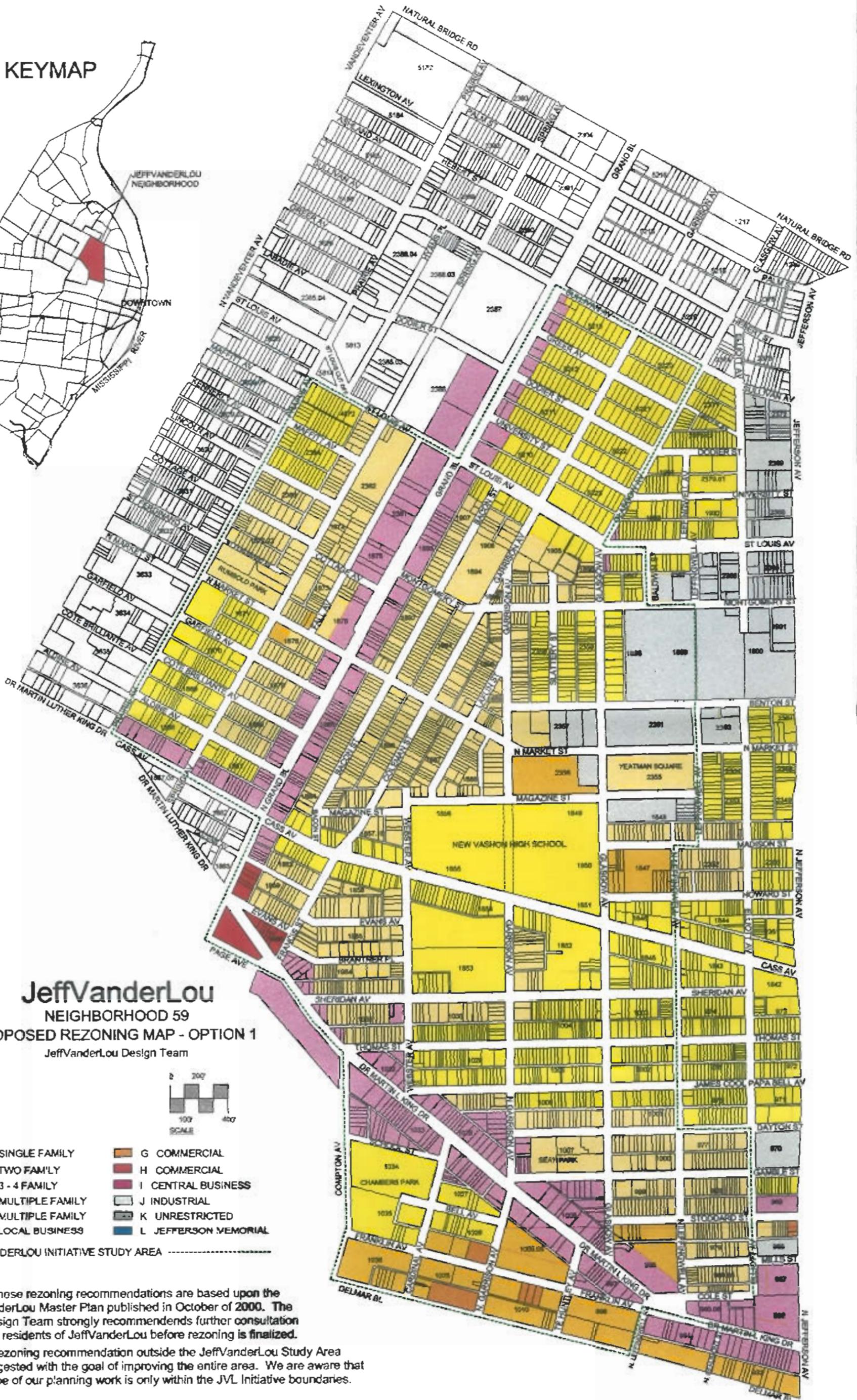
Contacts for more information on TNDs:

George Adams, Austin, Texas

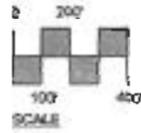
Reza Reyazi - planner with City of Columbus, Ohio

Elizabeth McCoury, City of Suffolk, Virginia

KEYMAP



JeffVanderLou NEIGHBORHOOD 59 PROPOSED REZONING MAP - OPTION 1 JeffVanderLou Design Team



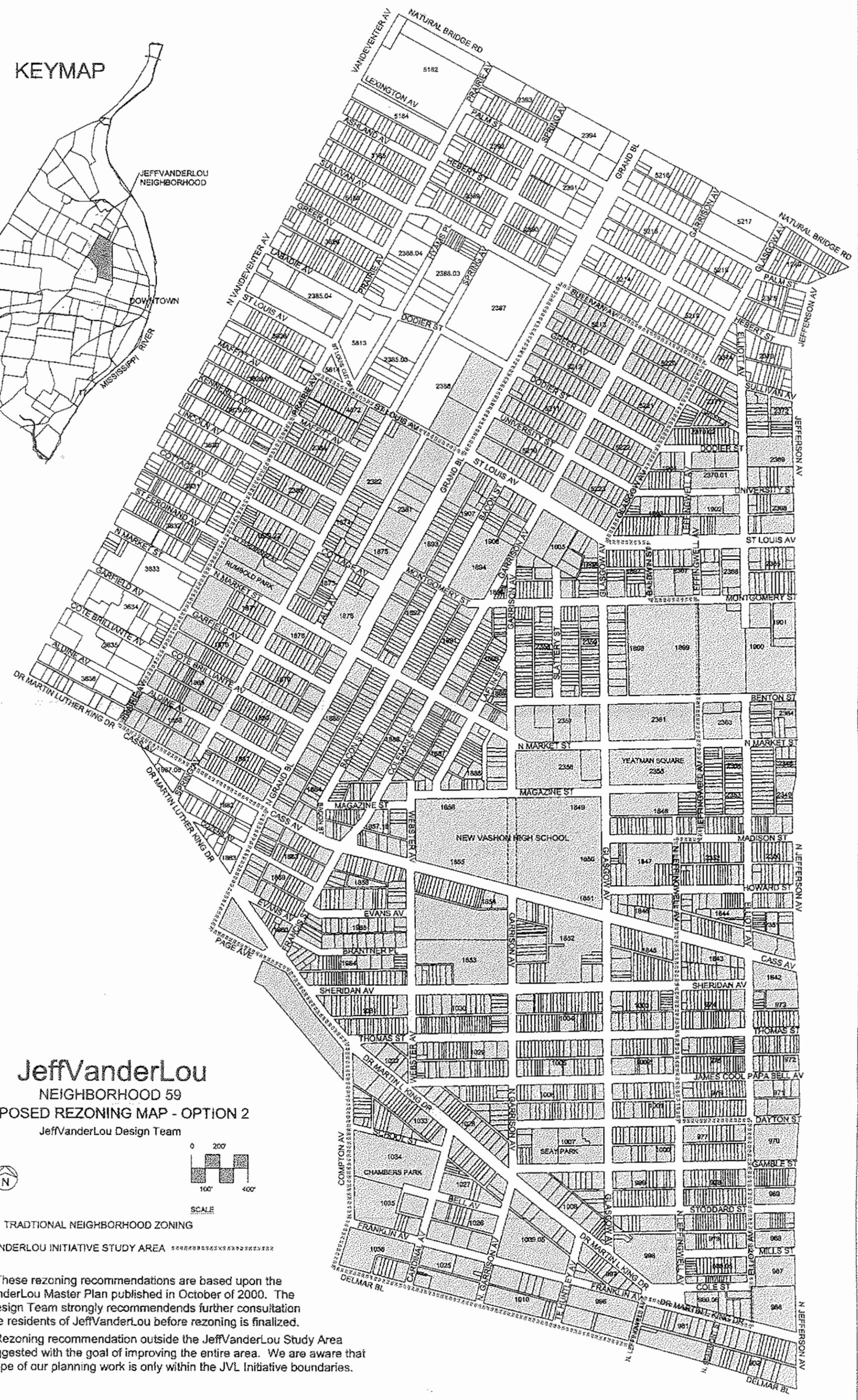
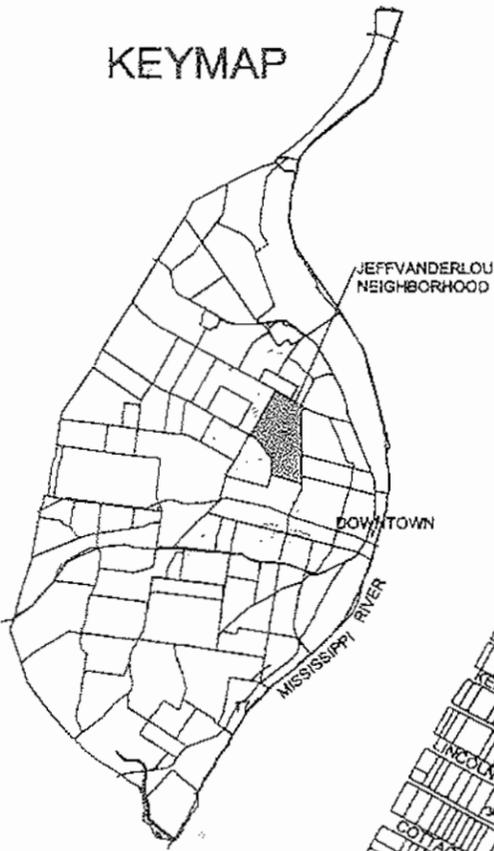
- | | |
|-------------------|----------------------|
| A SINGLE FAMILY | G COMMERCIAL |
| B TWO FAMILY | H COMMERCIAL |
| C 3-4 FAMILY | I CENTRAL BUSINESS |
| D MULTIPLE FAMILY | J INDUSTRIAL |
| E MULTIPLE FAMILY | K UNRESTRICTED |
| F LOCAL BUSINESS | L JEFFERSON MEMORIAL |

JEFFVANDERLOU INITIATIVE STUDY AREA - - - - -

Note: These rezoning recommendations are based upon the JeffVanderLou Master Plan published in October of 2000. The JVL Design Team strongly recommends further consultation with the residents of JeffVanderLou before rezoning is finalized.

Note: Rezoning recommendation outside the JeffVanderLou Study Area are suggested with the goal of improving the entire area. We are aware that the scope of our planning work is only within the JVL Initiative boundaries.

KEYMAP

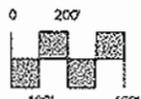


JeffVanderLou

NEIGHBORHOOD 59

PROPOSED REZONING MAP - OPTION 2

JeffVanderLou Design Team



SCALE

TRADITIONAL NEIGHBORHOOD ZONING

JEFFVANDERLOU INITIATIVE STUDY AREA

Note: These rezoning recommendations are based upon the JeffVanderLou Master Plan published in October of 2000. The JVL Design Team strongly recommends further consultation with the residents of JeffVanderLou before rezoning is finalized.

Note: Rezoning recommendation outside the JeffVanderLou Study Area are suggested with the goal of improving the entire area. We are aware that the scope of our planning work is only within the JVL Initiative boundaries.

Tab L

Infrastructure/ Public Facilities Impacts

Street Changes: Damaged streets and alleys will need to be repaired. Street changes at Webster, Garrison, Grand, Dr. Martin Luther King Jr. Drive, St. Louis, etc. will necessitate changes to the configuration of some streets. This will involve the building of medians and bulb outs. Some streets, such as Cote Brillante and Webster would be extended, involving the construction of new stretches of streets. In general, however, the street grid is to remain intact, while changes and additions are to be made to it to improve its traffic patterns and aesthetics.

Lighting: New development would require more street lighting than presently exists.

Larger Development on Dr. Martin Luther King Jr. Drive: Larger buildings planned for Dr. Martin Luther King Jr. Drive could possibly affect many different aspects of the infrastructure. Streets may need new curb cuts and turning lanes. Sewer, Gas and Electric service may need to be expanded to handle larger volumes.

Water: Existing damaged and/or aging water mains will need to be replaced before significant new development is begun. Water pressure north of Cass needs to be improved. Without these precautions, we could take the risk of being forced to fix a major problem later after new housing has been built.

Gas, Electric, Sewer: All of these utilities have the infrastructure in place to service these neighborhoods.

Public Facilities Impacts

Schools: Vashon High School, Columbia School and Dunbar School would be physically linked by street improvements on North Garrison Ave. Additionally, enhancements would be made to the land west of Dunbar, transforming a junkyard into park land. Neither the facilities nor the curricula of these institutions is addressed and so would not be affected by the Master Plan.

Churches: The Master Plan should not negatively affect Churches in the area. Positive effects would be improved streets and the possibility to increase membership of neighborhood congregations as the population in the area rises.

Recreation Centers: The planning team understood that it is clearly the desire of the neighborhood to maintain and expand activities at the Gamble Community Center. The renovation and expansion of this facility as well as the creation of supplementary activities in the adjacent Norman Seay Park are priorities for the JeffVanderLou Initiative. As improvements are made to Chambers Park, it is possible that the public swimming pool might be positively affected.

Parks: The Master Plan calls for the enhancement of Rumbold, Yeatman, Seay and Chambers Park. Additionally, the park North of Dunbar is to be expanded according to the plan. Park enhancements would include new tree plantings, new activities areas and improved lighting.

Libraries: The St. Louis Public Library does not currently have a branch in this neighborhood. The planning team would encourage library officials to consider JeffVanderLou for future branch expansion plans.

Forestation: The Master Plan does not call for the removal of existing trees in the neighborhood. It does call for the planting of dozens of new trees throughout the area along streets and in the parks.

Tab M

Environmental Compatibility

A Phase I Environmental Site Assessment of the JeffVanderLou planning area was completed in April 2000 by Professional Environmental Engineers, Inc. (PE) of St. Louis. This Phase I study will be consulted as new developments are planned. A list of findings, and maps, are included in the Appendix.

The PE report researched street directories and Sanborn Maps to determine whether, based on use or type of facility, the potential for adverse environmental conditions may exist. This research revealed several previous or current commercial facilities that based on use might need to be further investigated prior to development. These uses include automotive, junk yards, industrial, and dry cleaning, and were or are primarily located along major streets.

The report also relied on federal and state environmental databases for information. A few sites of concern were located within the planning area. Several others were indicated to be cleaned up or lie outside the planning area.

Finally, the report relied on reconnaissance throughout the neighborhood. A visual inspection of the area identified sites that may exhibit a potentially hazardous condition (such as leaking storage tanks, odors, petroleum products, and other characteristics).

Following is an excerpt from the Executive Summary of the report:

"The individual parcels of property within the study area are in various forms of development. Many parcels consist of improved lots for residential, commercial, or industrial purposes. Other property consists of vacant lots, lots with vacant or unused buildings, and unused lots with demolished or partially-demolished structures. Based on the historical information obtained for the subject study area, all parcels of property within the study area are believed to have been developed at one time or another.

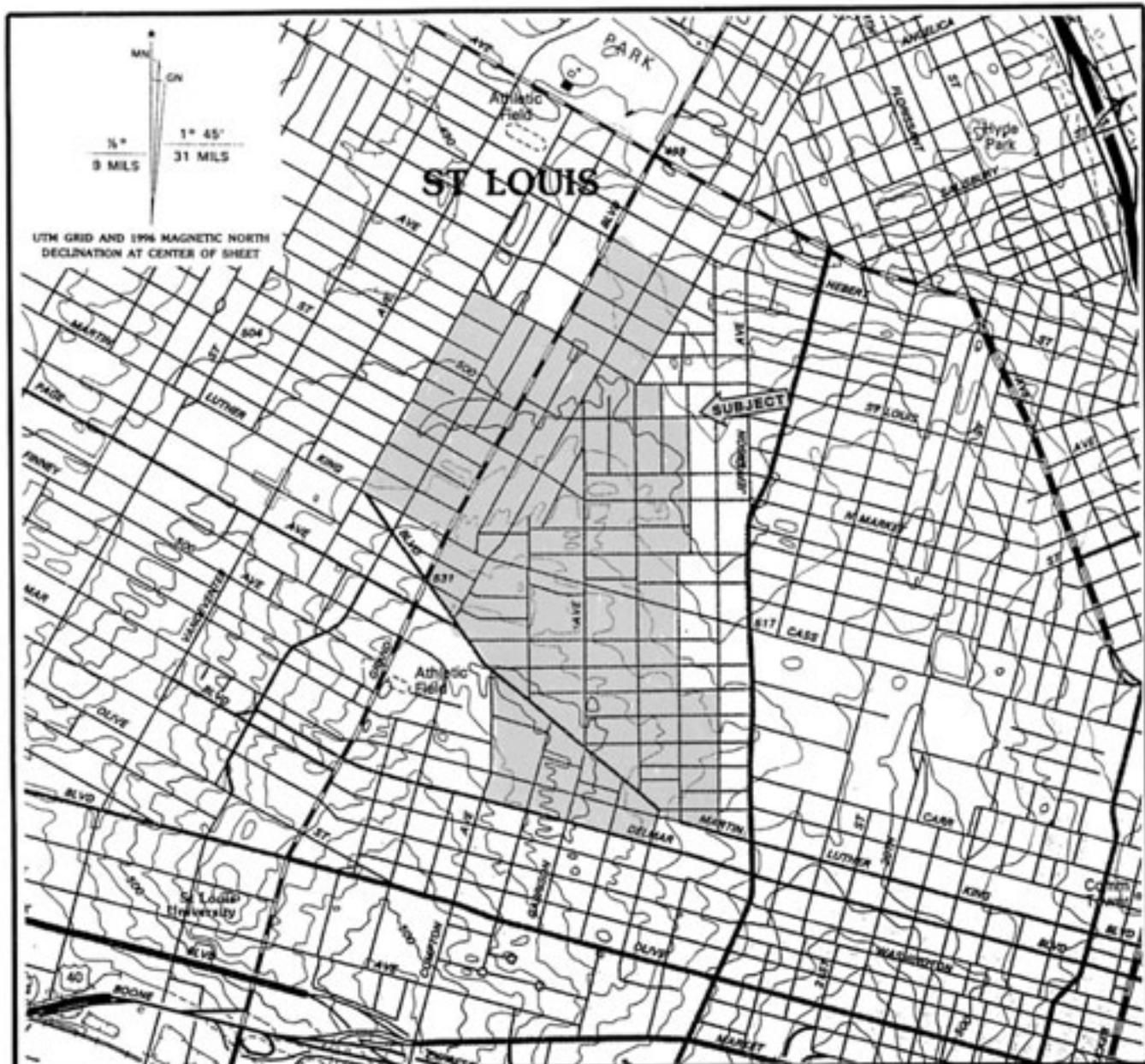
"In accordance with ASTM Standard E 1527-97, this Phase I Environmental Site Assessment included reconnaissance of the subject and adjoining properties, review of historical records, and review of environmental regulatory databases in an effort to identify evidence of recognized environmental conditions that may impact the property. This assessment has revealed 30 on-site and 12 nearby off-site facilities as having evidence of recognized environmental conditions in connection with the subject property.

"In addition, many facilities/properties contained environmental conditions that were questionable, or inconclusive, due to the limited available information and the methodologies employed in this investigation. PE was uncomfortable classifying these questionable sites as having recognized environmental conditions or not. As a result, a new terminology was developed to reflect environmental risks posed by

these questionable conditions. The term *probable environmental conditions* was used to describe situations that PE believed would be RECs if additional information were obtained. The term *possible environmental conditions* was used to describe situations that PE believed would be non-RECs if additional information were obtained.

"Another 37 on-site and 21 nearby off-site facilities were determined to have probably environmental conditions. In addition, PE was able to identify a total of 34 on-site and 20 nearby off-site facilities as having possible environmental conditions. It should be noted that either one of the probable or possible environmental conditions, as defined above, could be easily upgraded (to RECs) or downgraded (to non-RECs) if additional information were obtained. As a result, PE recommends that a more detailed investigation be conducted at all sites with questionable environmental conditions prior to development activities or negotiations involving a transfer of property at these sites.

"In addition ...it should be noted that asbestos-containing building materials, lead-based paint, and hazardous substances might be present throughout the subject study area. Although the mere presence of these materials are not considered to be recognized environmental conditions, the abatement and disposal of such items could be costly and should be taken into account prior to any development activities."



UTM GRID AND 1996 MAGNETIC NORTH DECLINATION AT CENTER OF SHEET

1/4" = 3 MILS
1" = 31 MILS



SITE LOCATION

Scale



FIGURE 1

SITE LOCATION MAP
Vashon/JVL Initiative Study Area
St. Louis, Missouri

Prepared For
McCormack Baron & Associates

Reference:

7.5 Minute Topographic Map of
Granite City, Illinois-Missouri (Date:
1993)



**PROFESSIONAL
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TABLES

TABLE 1
SUMMARY OF RECOGNIZED ENVIRONMENTAL CONDITIONS
 (Refer to Figure 3 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
1	2900-32 Cass Ave 2920-26 Cass Ave 2920 Cass Ave	West. Sup. Factory/Machine Shop West. Supplies/Superior Cutting Die Superior Cutting Die Co Tool Mfrs	89, 95 Sanborn 32, 50 Sanborn 75, 65, 55, 46, 36 Str. Dir.	On Site	Possible USTs, hazardous material storage area, poor housekeeping, etc.
2	2953 Cass Ave	Bologna filling sta	1936 Street Directory	On Site	In Vashon HS re-develop. area.
3	3000 Cass Ave 3000 Cass Ave 3000 Cass Ave	Patterson Service Station gas sta Filling Station Lewton filling sta	1965, 55 Street Directory 32, 50 Sanborn 1936 Street Directory	On Site	Building has been demolished; 3 to 6 USTs still exist on site along with associated piping.
4	1445-51 Garrison Ave 1445 Garrison Ave 3653 Cass Ave 3653 Cass Ave 3653-55 Cass Ave	Filling Station/Auto Repair Oscar Sandweiss auto repair Henry Fischer filling sta Springdale Serv Sta gas Spring & Cass Fina Service Station	32, 50 Sanborn 1955, 46, 36 Str. Dir. 1936 Street Directory 1955 Street Directory 1965 Street Directory	On Site	Current auto repair facility; very messy w/poor housekeeping. USTs are probably still present.
5	3749-53 Cass Ave 3751 Cass Ave 3753 Dr. Min Luther K Dr 3751 Cass Ave	Filling Station Ed Grempler filling sta Amoco Station No 242629 Bonds Auto Repair	1989 Sanborn 1936 Street Directory UST list Site Recon	On Site	Pump islands (for leaded gas) still present on site. UST listed as being closed in place ?
6	2601 Dayton	Melory Horn filling sta	1936 Street Directory	Off Site	
7	2844 Delmar Blvd 2844-46 Delmar Blvd 2844 Delmar Blvd	Willie Strain Service Station gas sta Filling Station Henry Clay filling sta	1965 Street Directory 1950 Sanborn 1946 Street Directory	Off Site	Currently residential property.
8	3110 Delmar Blvd 3110 Delmar Blvd 3110 Delmar Blvd	Robert Smith Auto Repair ZZYX Oil Co Mars Oil Co gas sta	1955 Street Directory UST list 1975, 65 Street Directory	Off Site	Currently a liquor store. Three USTs supposedly removed on site.
9	1023 Elliot Ave 1023 Elliot Ave 1023 Elliot Ave 2703 Mills St.	Bill's Serv Sta gas Filling Station Mitchell Cab Co Elliot & Mills	1965 Street Directory 1950 Sanborn 1946 Street Directory UST list	On Site	Currently a vacant lot. Two USTs supposedly closed in place.
10	1316-18 Elliot Ave	Pearline filling sta	1936 Street Directory	Off Site	
11	3147 Evans Ave	Jone's Fuel Oil Co	1965 Street Directory	On Site	Currently Keys Auto Repr.
12	2803 Franklin Ave 2801-05 Franklin Ave 2803 Franklin Ave 2803 Franklin Ave	Rice Serv Sta Filling Sta Jamison gas sta Standard Oil Co fill sta	1965 Street Directory 1950 Sanborn 1955, 46 Street Directory 1936 Street Directory	On Site	

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 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
13	700-04 Garrison Ave	George Murphy gas sta	1955 Street Directory	Off Site	
	700-04 Garrison Ave	Atkins filling sta	1946 Street Directory		
	700-06 Garrison Ave	Larmie filling sta	1936 Street Directory		
14	815 Garrison Ave	Laclede Cleaning Co	1955 Street Directory	On Site	May be facility located at 3001 Delmar (MO Electric).
	815 Garrison Ave	Frank Scott clo clnr	1946, 36 Street Directory		
15	1700 Garrison Ave	American Oil Co	1936 Street Directory	On Site	Currently being re-developed as part of the Vashon High School project.
	1710 Garrison Ave	Metro Spick & Span Clnrs	1936 Street Directory		
	1710 Garrison Ave	Tool & Die Mfg	89, 95, 97, 99 Sanborn		
	2431 Garrison Ave	Smith Auto Repair	1975 Street Directory		
	2427-31 Garrison Ave	Filling Station	50, 89 Sanborn		
16	2431 Garrison Ave	Wedge Serv Sta gas sta	1955 Street Directory	On Site	Vent pipes and UST dispenser still present on site (USTs are likely to be present also). Lot is very messy w/poor house-keeping. Currently Watson Auto Repair.
	2431 Garrison Ave	Thos Wesley filling sta	1946 Street Directory		
	2431 Garrison Ave	Creath Bros filling sta	1936 Street Directory		
	2424 Laflin Ave	Auto Repr	50, 95, 97 Sanborn		
	1500 Glasgow Ave	Star Service Station	1975 Street Directory		
	1500-10 Glasgow Ave	Filling Station	89, 95, 97 Sanborn		
	1500 Glasgow Ave	Abandoned Station	LUST list		
18	1230 N. Grand Blvd	Filling Station ?	1932 Sanborn	Off Site	
	1230 N. Grand Blvd	Used Auto Sales	1950 Sanborn		
19	1444-50 N. Grand Blvd	Filling Station	1950 Sanborn	On Site	Currently Do Nut Shop; has bay doors reminiscent of ser. sta.
	1446 N. Grand Blvd	StCyr filling sta	1936 Street Directory		
20	1500 N. Grand Blvd	Pete's Service Station gas sta	75, 65, 55 St. Directory	On Site	Currently a vacant lot with no signs of USTs or former service station. Three USTs are listed as being removed from site.
	1500 N. Grand Blvd	Filling Station	50, 95, 97, 99 Sanborn		
	1500 N. Grand Blvd	Pete's Service Station	UST list		
	1500 N. Grand Blvd	Ed Chill filling sta	1936 Street Directory		
	1500 N. Grand Blvd	Filling Station	89, 95, 99 Sanborn		
21	1501-11 N. Grand Blvd	Joseph Becker Wall Paper & Paint Co	1975 Street Directory	On Site	3 or more USTs/piping present. UST supposedly closed in place.
	2515 N. Grand Blvd	Paint Factory	89, 95, 99 Sanborn		
22	2515-21 N. Grand Blvd	Howard's Cleaners (main ofc)	1955, 46, 36 Str. Dir.	On Site	
	2515 N. Grand Blvd	Howard's Cleaners	1955, 46 Str. Dir.		
	2512 Fall Ave	Joseph Becker Wall Paper & Paint Co	UST list/Site Recon		

TABLE 1
SUMMARY OF RECOGNIZED ENVIRONMENTAL CONDITIONS
 (Refer to Figure 3 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
23	2520 N. Grand Blvd	Grand Service Station	1955 Street Directory	On Site	Currently south end of Grand Motel property.
	2520 N. Grand Blvd	Filling Station	1950 Sanborn		
	2520 N. Grand Blvd	Pregalin filling sta	1946 Street Directory		
	2520 N. Grand Blvd	Spies filling sta	1936 Street Directory		
24	2555 N. Grand Blvd	Nick's Catering	Site Recon	On Site	UST on site - probable release.
	2900-10 N. Grand Blvd	Filling Station	1950 Sanborn	On Site	Currently residential property.
25	2904 N. Grand Blvd	Lang filling sta	1936 Street Directory	On Site	Currently commercial building on site.
	2910 N. Grand Blvd	Harry's Serv Sta gas sta	1955, 46, 36 Str. Dir.		
	3016 N. Grand Blvd	Peerles Lacquer & Chemical Co	1936 Street Directory		
	3016 N. Grand Blvd	Gasoline station	1950 Sanborn		
	3016 N. Grand Blvd	Bob King's Mobil Serv Sta	1965 Street Directory		
	3016 N. Grand Blvd	Stoehner Tire & Serv Sta	1955 Street Directory		
	3016 N. Grand Blvd	Rueckert & Stoehner filling sta	1946 Street Directory		
	3052 Madison	Auto & Fuel	1950 Sanborn		
26	2723 Dr. Mtn Luther K Dr	Fourty Minute Cleaners	75 St Dir/FINDS/R-SQG	On Site	In Vashon HS redev. area.
	2723-27 DMLK Drive	Dry Cleaning	89, 95 Sanborn	On Site	Currently a dry cleaning facility.
	2723-27 Franklin Ave (*)	Fourty Minute Cleaners	1965 Street Directory	On Site	Currently vacant property.
	2733 Franklin Ave (*)	Harper's Cleaners clo clnr	1965, 55 Street Directory		
27	2959-65 DMLK (Easton)	Filling Station	1950 Sanborn	On Site	Currently vacant property.
	2963 (?) DMLK (Easton)	Filling Station	1932 Sanborn	On Site	Currently the Hair Clinic; USTs.
28	3075 DMLK Dr (Easton)	Filling Station	32, 50 Sanborn	On Site	Currently a vacant lot.
	3139 DMLK Dr (Easton)	Filling Station	32, 50 Sanborn	On Site	Abandoned commercial buildings on site.
29	3150 Dr. Mtn Luther K Dr	New Regal Cleaners	1975 Street Directory	On Site	Currently Easton Tire Co.; UST fill port & vent north of building.
	3150 Dr. Mtn Luther K Dr	Dry Cleaning	89, 95, 99 Sanborn	On Site	Currently a vacant lot.
30	3213 DMLK Dr (Easton)	Battery Service	32, 50 Sanborn	On Site	Currently vacant property.
	3148-58 Sheridan Ave	Tire Service	50, 89, 95 Sanborn	On Site	Currently the Hair Clinic; USTs.
31	3520 DMLK Dr (Easton)	Filling Station	1950 Sanborn	Off Site	Currently a vacant lot.
	2708 North Market	Motor-Moor Auto Serv reprs	1955 Street Directory	Off Site	
32	2700-10 North Market	Filling Station	1950 Sanborn	Off Site	
	2708 North Market	Varner filling sta	1946 Street Directory	Off Site	
33	2708 North Market	Fred Reimers filling sta	1936 Street Directory	Off Site	
	1507 Prairie Ave	Leslie Wencker/Jos Jacques fill sta	1936 Street Directory	Off Site	

TABLE 1
SUMMARY OF RECOGNIZED ENVIRONMENTAL CONDITIONS
 (Refer to Figure 3 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
37	2900 Prairie Ave	Marshall's Service Sta	1955 Street Directory	Off Site	
	2900 Prairie Ave	Schau's filling sta	1946 Street Directory		
	2900 Prairie Ave	Polzin Service Stations filling sta	1936 Street Directory		
38	3224 St. Louis Ave	Bianco Bros Towing & Auto Repair	1965 Street Directory	On Site	Currently a vacant lot.
	3224 St. Louis Ave	Bianco Bros Auto Repair	1955 Street Directory		
	3224 St. Louis Ave	Bianco Bros filling sta	1946 Street Directory		
39	3100 Sheridan Ave	Rice's Auto Repair Shop	1975 Street Directory	On Site	Pump island & vent pipes for USTs still present; USTs are believed to exist on site.
	3100 Sheridan Ave	Filling Station	1950 Sanborn		
	3100 Sheridan Ave	M & J Service Sta	1955 Street Directory		
	3100 Sheridan Ave	Robt Nelson filling sta	1936 Street Directory		
40	2437-39 Spring Ave	Dry Cleaning	89, 95 Sanborn	On Site	3 USTs present on site; very old messy site w/ signs of release.
	2437-39 Spring Ave	Spring Cleaning Co Clo clns	75, 65, 55 St. Directory		
41	2840 Spring Ave	AFC Industries	CERC-NFRAP list	Off Site	4 to 5 USTs present on west end; old industrial site.
	2840 Spring Ave	Carter Carburator	CERCLIS/FINDS lists		
42	2841 Spring Ave	Federal-Mogul Corp	LUST/UST lists	Off Site	Industr. Facility w/ USTs.

Notes:

All facilities are located on Figure 3 - Recognized Environmental Conditions Map; map numbers on table correspond to those shown on Figure 3.

TABLE 2
SUMMARY OF PROBABLE ENVIRONMENTAL CONDITIONS
 (Refer to Figure 4 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
1	3749 Aldine Ave	Mack Corp Truck Bldrs DeMac Equipment	1975 Street Directory UST List/Site Recon	On Site	No signs of UST on outside of building.
2	2940 Benton St 2940 Benton St	Buxton & Skinner Printing Co. Former Lithographing Co.	1975 Street Directory Site Recon	On Site	Old building w/ lots of pipes & vents sticking out.
3	2717 Cass Ave	Martin's Cleaners	1975 Street Directory	Off Site	
4	2704 - 2732 Cass Ave 2700-30 Cass Ave 2718 Cass Ave 2718-26 Cass Ave 2716-18 Cass Ave 2716-18 Cass Ave 2724 Cass Ave	Auto Wrecking/Junk Yard Auto Wrecking/Junk Yard Continental Auto Parts & Salvage Co. Continental Auto Parts & Salvage Co. Continental Auto Salvage Co. Friedman Auto Salvage Co Alf How filling station	1999 Sanborn 1950 Sanborn 1975 Street Directory 1965 Street Directory 1946 Street Directory 1936 Street Directory 1936 Street Directory	Off Site	All parcels/properties owned and managed currently by one operator as a junk yard.
5	2733-37 Cass Ave	Auto Service	89, 97 Sanborn	Off Site	Currently Cont. Auto Salvage.
6	2745 Cass Ave	Moon Cleaners	1946 Street Directory	Off Site	Current parking lot.
7	2937 Cass Ave 2937 Cass Ave 2939 Cass Ave 2937-39 Cass Ave	Wilson Auto Body Shop auto repr Auto Repr Auto Body Shop Wilson Auto Body Shop	1975, 55 Street Directory 89, 95, 97, 99 Sanborn 89, 95, 97 Sanborn 1965 Street Directory	On Site	Currently being re-developed as part of the Vashon High School project.
8	3114-18 Cass Ave 3120 Cass Ave 3124 Cass Ave 3136a Cass Ave 3136R Cass Ave	Specialized Motor & Body Serv Auto Repr Am Auto Radiator Repr LaGalio auto repr Auto Repr and junk yard	1955 Street Directory 1932 Sanborn 1936 Street Directory 1946, 36 Street Directory 50, 89, 95 Sanborn	On Site	Currently used as an auto parts/junk yard facility.
9	3500 Cass Ave	Fresno Clin & Hatters Clo Clin	1965 Street Directory	On Site	Currently a vacant lot.
10	3600-30 Cass Ave 3628 (& 3705) Cass Ave 3630 Cass Ave 3650 Cass Ave	Auto Wrecking/Junk Yard Liberty Auto Salvage Co. Liberty Auto Salvage Co. Liberty Auto Salvage Co.	1999 Sanborn 75, 65, 55 St. Dir./FINDS 1946 Street Directory 1936 Street Directory	Off Site	
11	3705-19 Cass Ave 3729-41 (?) Cass Ave	Junk Yard Junk Yard	89, 95, 99 Sanborn 89, 99 Sanborn	On Site	Currently one property.

TABLE 2
SUMMARY OF PROBABLE ENVIRONMENTAL CONDITIONS
 (Refer to Figure 4 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
29	1551 N. Grand Blvd	Lewis's Clark Service Station	1975 Street Directory	On Site	Vacant commercial property.
30	1715 N. Grand Blvd	Dry Cleaning	89, 95 Sanborn	On Site	Vacant lot?
	1715 N. Grand Blvd	Correct Cleaners	1955, 46 Street Directory		
31	1824 N. Grand Blvd	Stras Cleaners	1965 Street Directory	On Site	Currently Key Service.
	1824 N. Grand Blvd	Stras Cleaning & Dyeing Co	1955, 46 Street Directory		
	1828 N. Grand Blvd	Mid-City Lndry & Cing	1955 Street Directory		
32	1921 N. Grand Blvd	Stras Cleaning & Dyeing Co	1946, 36 Street Directory	On Site	Abandoned/unused property.
33	2437 N. Grand Blvd	Winston Cleaners clo clns	1965 Street Directory	On Site	
	2439 N. Grand Blvd	StClair Curtain Clnrs	1955 Street Directory		
34	2503 N. Grand Blvd	Spick & Span Clnrs	1946 Street Directory	On Site	Currently Grand U-Buy Store.
	2503 N. Grand Blvd	Ford Cleaning Co	1936 Street Directory		
35	2525 N. Grand Blvd	Davis Cleaners	1946 Street Directory	On Site	Same as 2515 Grand?
36	2540 N. Grand Blvd	Silas & Sons Auto Repair	1975 Street Directory	On Site	Currently north end of Grand Motel property.
	2540 N. Grand Blvd	Auto Service	1950 Sanborn		
	2540 N. Grand Blvd	Slatton's Standard Auto Repair & Serv	1965 Street Directory		
37	3000 N. Grand Blvd	Cook Paint & Varnish Co	1936 Street Directory	On Site	Paved lot with evidence of former commercial/industrial facility on site.
	3008 N. Grand Blvd	Lang Motor Services auto repr	1946 Street Directory		
	3008 N. Grand Blvd	Beckring Auto Body Co	1936 Street Directory		
	3000-10 N. Grand Blvd	Vacant lot	Site Recon		
38	3119 N. Grand Blvd	Starlight Cleaners	1975 Street Directory	Off Site	
	3125 N. Grand Blvd	Charles Quality Cleaners clo clns	1955 Street Directory		
	3125 N. Grand Blvd	Lungstras Dyeing & Cing Co	1936 Street Directory		
39	817 N. Leffingwell Ave	Denos Bros clnrs	1946 Street Directory	On Site	Currently new apt. complex.
	817 N. Leffingwell Ave	Leffingwell Cleaners	1936 Street Directory		
	821 N. Leffingwell Ave	Vaski Cleaners	1955 Street Directory		
40	2408 N. Leffingwell Ave	Homer Doerr & Sons Plating Co.	1975 Street Directory	Off Site	
	2408 N. Leffingwell Ave	Homer Doerr & Sons Plating Co.	FINDS		
41	1001-07 Jefferson	Police Station	Site Recon	Off Site	UST pump island present.
42	3868 Lincoln Ave	Clemens Clnrs clo clnrs	1955 Street Directory	Off Site	Approximate location on map.
	2525 Vandeventer	Amoco Sta 16691	UST list		
43	2839 Madison	New Regal Clns clo cln	1965 Street Directory	On Site	Currently vacant/residential property.
	2839 Madison	Reliable Cleaners	1955 Street Directory		

TABLE 2
SUMMARY OF PROBABLE ENVIRONMENTAL CONDITIONS
 (Refer to Figure 4 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
44	3047-63 Madison 1528 Webster	Junk Yard Eugene & JC Auto Repair	1995 Sanborn 1975 Street Directory	On Site	In Vashon HS redev. area.
45	3702 Maffitt Ave 3706 (?) Maffitt Ave	North-Side Cleaners clo cln Auto Repr	1955 Street Directory 1995 Sanborn	On Site	Currently residential property.
46	2800-30 Magazine 2830 Magazine	Sterling Aluminum Products Inc Bee Cabinet Corp	1950 Sanborn RCRIS-SQG/FINDS	On Site	Newer building in front of old building; drum stor. area pres.
47	2608 Franklin Ave (*)	Howard's Cleaners Inc	1946 Street Directory	Off Site	
48	2626 Franklin Ave (*)	Sam Denos clo clnr	1965, 55, 46, 36 Str Dir	Off Site	
49	3323 DMLK Dr.	My T Mike's Automotive	Site Recon	On Site	Waste oils spilled along bldg?
50	3620 DMLK Dr (Easton)	Junk Yard	Site Recon	Off Site	Part of Liberty Auto Salvage?
51	3727 Dr. Mtn Luther K Dr 3727 Dr. Mtn Luther K Dr 3753 Dr. Mtn Luther K Dr	Gus Auto Repair Junk Yard Amoco Station # 24629	1975 Street Directory 1999 Sanborn UST list	Off Site	Part of Liberty Auto Salvage property?
52	2900 North Market 2900-24 North Market 2914-16 North Market	Kramer filling sta Sterling Aluminum Products Inc Sterling Aluminum (fdry)	1936 Street Directory 1950 Sanborn 1955, 46 Street Directory	On Site	Currently Wood's Printing.
53	2925 North Market	Sterling Aluminum Products Inc	1955, 46 Street Directory	On Site	Currently Eikay Industries.
54	3012-14-16 North Market 3012-14 North Market 3012-14-16 North Market	Omar Clns clo cln Dry Cleaning Rose White Cleaners	1965, 55 Street Directory 1950 Sanborn 1946 Street Directory	On Site	Currently a vacant lot.
55	3508 North Market 3510 North Market 3508-10 North Market	S-Quire Clnrs & Dyers Jas Owens clo clnr Sterling Lacquer Mfg Co	1955 Street Directory 1955 Street Directory 1946 Street Directory	On Site	Currently a commercial facility.
56	2756 Prairie Ave	Prairie Tailors & Clnrs	1955 Street Directory	On Site	Currently residential property.
57	3204 St. Louis Ave 3204 St. Louis Ave 3204 St. Louis Ave 3204 St. Louis Ave	Eddie Taylor Auto Repair Paints & Auto Repairs Central Auto Repair Independent Service Co Inc auto reprs	1975 Street Directory 89, 95, 97 Sanborn 1965 Street Directory 1955, 46, 36 Str. Dir.	On Site	Currently Hair Care Designs.
58	3055 (?) Sheridan Ave 3057 Sheridan Ave	Junk Yard Weaver's Auto Repair	89, 99 Sanborn 1975 Street Directory	On Site	Currently a junk yard.

Notes:

All facilities are located on Figure 4 - Probable Environmental Conditions Map; map numbers on table correspond to those shown on Figure 4.

TABLE 3
SUMMARY OF POSSIBLE ENVIRONMENTAL CONDITIONS
 (Refer to Figure 5 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Location	Notes
1	3723 Aldine Ave	Auto Junk Yard	89, 95, 99 Sanborn	On Site	Currently residential.
2	3740 Aldine Ave	Junk Yard	89, 99 Sanborn	On Site	Paved lot w/ auto body parts.
3	2500-2616 (?) Baldwin 2526 Baldwin	Warner-Jenkinson Co Warner-Jenkinson Div of Univ. Food	89, 95 Sanborn FINDS/RCRIS-LOG/UST TRIS/TSCA Lists 1936 Street Directory	On Site/Off Site	Large industrial facility viewed from perimeter only; 55-gallon drums of unknown substances present. Facility is well kept; 8 USTs supposedly removed.
	2520-28 Baldwin	Warner-Jenkinson Mfg Co extracts			Currently a vacant lot.
4	3007 Bell Ave 3007 Bell Ave	Bell Auto Repair Kimbrough auto repr	1955 Street Directory 1946 Street Directory	On Site	
	1003 Garrison	Kimbrough auto repr	1936, 46 Street Directory		
5	3158 Branter Pl	Auto Repr	1932 Sanborn	On Site	Currently residential.
6	2742 Cass Ave	Vito D'Angelo clo clnr	1936 Street Directory	Off Site	Current Fish House market.
7	2954 & 2962 Cass Ave 2954-56 Cass Ave 2954-56 Cass Ave 2962-64 Cass Ave	Lambert L A Plating Co. Plating Wks Lambert L A Plating Co. Acme Tool & Die Mfg Co	1975 Street Directory 50, 89 Sanborn 1965, 55 Street Directory 1946 Street Directory	On Site	Currently commercial property with buildings on site and adjacent vacant lot.
8	3631-33 Cass Ave 3631 Cass Ave	Machine Shop Asphalt/Tar Company	1995 Sanborn Site Recon	On Site	Has tar trailer parked on adjacent property to north.
9	3703 Cass Ave 3703 Cass Ave	Mary Kogan rug clnr Tire Shop	1936 Street Directory Site Recon	On Site	Facility associated with junk yard to the west.
10	3648 Cote Brillante Ave	Auto Repr	1999 Sanborn	On Site	Currently vacant comm. Prop.
11	3726 Cote Brillante Ave	Auto Repr	95, 99 Sanborn	On Site	Currently residential property.
12	2916 1/2 Delmar Blvd 2918 Delmar Blvd	Westmoland Auto Repair Fuel Yard ?	1965 Street Directory 1950 Sanborn	Off Site	Currently residential property.
13	2933-2947 Delmar Blvd 2921-2941 Delmar Blvd	S. G. Adams Metalware Co. Delmar St Drums	1950 Sanborn CERCLIS-NFAP/FINDS	On Site	Currently residential property.
14	3001 Delmar Blvd 3001 Delmar Blvd	Missouri Electric Motor Co Elec. Supplies W. Ho.	FINDS/RCRIS-SQG 1989 Sanborn	On Site	Lots of pipes and vents sticking out of building (cleaners?).
15	1210 Elliot Ave 1210 Elliot Ave 1210 Elliot Ave 1210 Elliot Ave	Valentine Mtr Serv auto repr Auto Repr Abram & Son auto body repr Andrew Jackson Auto repr	1965, 55 Street Directory 1950 Sanborn 1946 Street Directory 1936 Street Directory	Off Site	Currently Chicago Trading, Inc., property.
16	1409 Francis	Aras Gavin clo clnr	1946 Street Directory	On Site	Currently vacant.

TABLE 3
SUMMARY OF POSSIBLE ENVIRONMENTAL CONDITIONS
 (Refer to Figure 5 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Type	Notes
17	2918 Franklin Ave	St. Louis Plating Co	FINDS list	On Site	Active (?) facility.
18	2607 Gamble	Davis Auto Storage & Garage	1955 Street Directory	Off Site	
	2607 Gamble	Polar Wave Ice & Fuel Co.	1909 Sanborn		
19	2607-09 Gamble	Elliot Davis auto repr	1936 Street Directory	Off Site	
	3807 Garfield Ave	Three Star Garage	1955 Street Directory		
20	1004 Glasgow Ave	Auto Repr	1932 Sanborn	On Site	Currently residential property.
	2821 Dr. Mtn Luther K Dr	Bolden's Auto Repair Shop	1975 Street Directory		
21	2823-29 DMLK (Easton)	Tin Shop	1909 Sanborn	Off Site	
	1303 N. Grand Blvd	Schaales Cleaners	1936 Street Directory		
22	1442-44 N. Grand Blvd	Acme Rug & Carpet Clns	1955, 46, 36 Str. Dir.	On Site	Commercial buildings present.
23	1518 N. Grand Blvd	Leon Boyer auto repr	1946 Street Directory	On Site	Vacant lot/residential property.
24	1802-04 N. Grand Blvd	Joe's Auto Repair	1955 Street Directory	On Site	Currently commercial property.
25	1817-19 N. Grand Blvd	Auto Repr	89, 95, 99 Sanborn	On Site	Currently French Welding Ser.
26	2557 ? N. Grand Blvd	Vacant burnt-out buildings	Site Recon	On Site	Commercial buildings in ruins.
27	2831-51 N. Grand Blvd	Carter Carburator	FINDS/RCRIS-LQG	Off Site	Currently Wilco Plastics Co., & parking lot for Carter Carburator; possible UST on Wilco property.
	2801-61 (?) N. Grand Blvd	Carter Carburator	1999 Sanborn		
28	3615 St. Louis Ave	Hubert Thompson	FINDS list	On Site	Currently residential property.
	2914-16 N. Grand Blvd	Jadco International	FINDS		
29	2914-16 N. Grand Blvd	PRTG	1989 Sanborn	Off Site	
	2916 N. Grand Blvd	Wilco Printing	1975, 65 Street Directory		
30	710-12 N. Leffingwell Ave	Wm Murray auto repr	1936 Street Directory	Off Site	
	814-18 N. Leffingwell Ave	Johnson garage	1946 Street Directory		
31	814-18 N. Leffingwell Ave	Dalles & Son auto repr	1936 Street Directory	On Site	Old building w/ lots of vents.
	2412 Laflin	Vacant (?) commercial facility	Site Recon		
32	3813 Lincoln Ave	Lincoln Auto Repair	1955 Street Directory	Off Site	Approximate location on map.
33	2929 Dr. Mtn Luther K Dr	Easton Ewing Auto Repair	1975 Street Directory	On Site	Currently residential property.
	2929 DMLK (Easton)	Auto Repair	1932 Sanborn		
34	3820 Dr. Mtn Luther K Dr	Alias Plumbing Supply Co	UST	Off Site	
35	3845 Dr. Mtn Luther K Dr	Auto Repr Painting	1995 Sanborn	Off Site	
	3845 Dr. Mtn Luther K Dr	Trim Auto Body Reprs	1975 Street Directory		
	3857-59 DMLK Drive	Auto Repr	1995 Sanborn		
	3857 Dr. Mtn Luther K Dr	Central Garage Auto Repair	1975 Street Directory		

SUMMARY OF POSSIBLE ENVIRONMENTAL CONDITIONS
 (Refer to Figure 5 for Graphic Display)
 Vashon/JVL Phase I ESA
 Project No. 6649

Map Number	Address	Name	Source	Type	Notes
36	3848 Dr. Mtn Luther K Dr	Jones Bros Auto Parts (Junk Yard)	1975 Street Directory	Off Site	
37	2601-? Mills	Commercial/Industrial Facility	Site Recon	Off Site	Old building w/ lots of vents.
38	2711 Montgomery	Harry Graser auto repr	1946, 36 Street Directory	Off Site	
39	3012 Montgomery	Aram Cherkorian clo cln	1955 Street Directory	On Site	Currently a vacant lot.
40	3503 Montgomery	Muskopf auto repr	1936 Street Directory	On Site	Currently Moore Auto Repair.
41	3501-03 Montgomery	Auto Service Repairing	89, 95, 97, 99 Sanborn	On Site	
42	2915 North Market	Industrial (?) Facility	1955, 46 Street Directory	On Site	Part of Buxton & Skinner?
43	1901-? Prairie Ave	Commercial facility ?	Site Recon	Off Site	
44	1905-07 Prairie Ave	Dana auto repr	1936 Street Directory	Off Site	
45	2611-13 Prairie Ave	Mel's Auto Service	1955 Street Directory	Off Site	
46	26-11 Prairie Ave	Auto Repr	1999 Sanborn	Off Site	
47	2611-13 Prairie Ave	Muskopf auto repr	1946, 36 Street Directory	Off Site	
48	3523 St. Louis Ave	Pahl Cleaners	1946 Street Directory	On Site	Currently a commercial facility.
49	3141 School St	School Street Drum Site	CERC-NFRAP	On Site	Currently houses a church.
50	2801 1/2 Sheridan Ave	Marcellus Hatch clo clnr	1946 Street Directory	On Site	Currently residential property.
51	2824 (?) Sheridan Ave	Auto Repr	1950 Sanborn	On Site	Currently a vacant lot.
52	2821 ? Spring Ave	Messy Industrial Lot	Site Recon	Off Site	May be part of 2841 Spring.
53	2809 Stoddard	Sary Peterson clo clnr	1946 Street Directory	On Site	Vacant lot/residential property.
54	2833 Stoddard	Wm Price clo cln	1955 Street Directory	On Site	Vacant lot/residential property.
55	2735 Thomas	Miller auto repr	1936 Street Directory	Off Site	Vacant lot/residential property.
56	1344 Webster Ave	Branch's Fuel & Ice Co.	1975, 65 Street Directory	On Site	
57	3123 Dr. Mtn Luther K Dr	Auto Repr	89, 95 Sanborn	On Site	Commercial buildings on site.

Notes:

All facilities are located on Figure 5 - Possible Environmental Conditions Map; map numbers on table correspond to those shown on Figure 5.

Tab N

Conformance with the City's Comprehensive Plan

The 1947 Comprehensive Plan of the City of St. Louis proposes the direction of future land use. It also contains general policies regarding long-term development, and addresses problems and goals through planning and development.

Density: The city of St. Louis comprehensive plan of 1947 projected a 10% increase in population from 1940 to 1970, estimating a population of over 900,000 by 1970. The City of St. Louis and the JeffVanderLou Neighborhood actually experienced a rapid decline in population during this period. The '47 plan did not foresee and thus did not address the massive areas of vacant land that currently exist due to this exodus.

The '47 Plan called for suburbanized, less dense development with deep suburban setbacks from streets. The JVL Master Plan calls for a return to the original density of the neighborhood.

Blighted and Obsolete Districts: The 1947 City Plan indicates that the northern portion (60%) of the JeffVanderLou neighborhood was considered a blighted district, while the lower southern portion, approximately 40%, was considered obsolete.

Deterioration of housing, which leads to the "blighted" designation, was attributed to juvenile delinquency, crime, disease and poverty. The '47 Plan, like the JVL Master Plan, called for rehabilitating the buildings in blighted areas.

According to the '47 Plan, an obsolete district is the result of blight. Thus obsolete housing was considered beyond rehabilitation, leaving land clearance as the only solution to relieve the problem. Although the JVL Master Plan recognized the need for removal of some derelict properties, it does not draw an "obsolete" blanket over 40% of the neighborhood, as does the '47 Plan. The JVL Master Plan calls for the rehabilitation of as many older structures as is economically feasible.

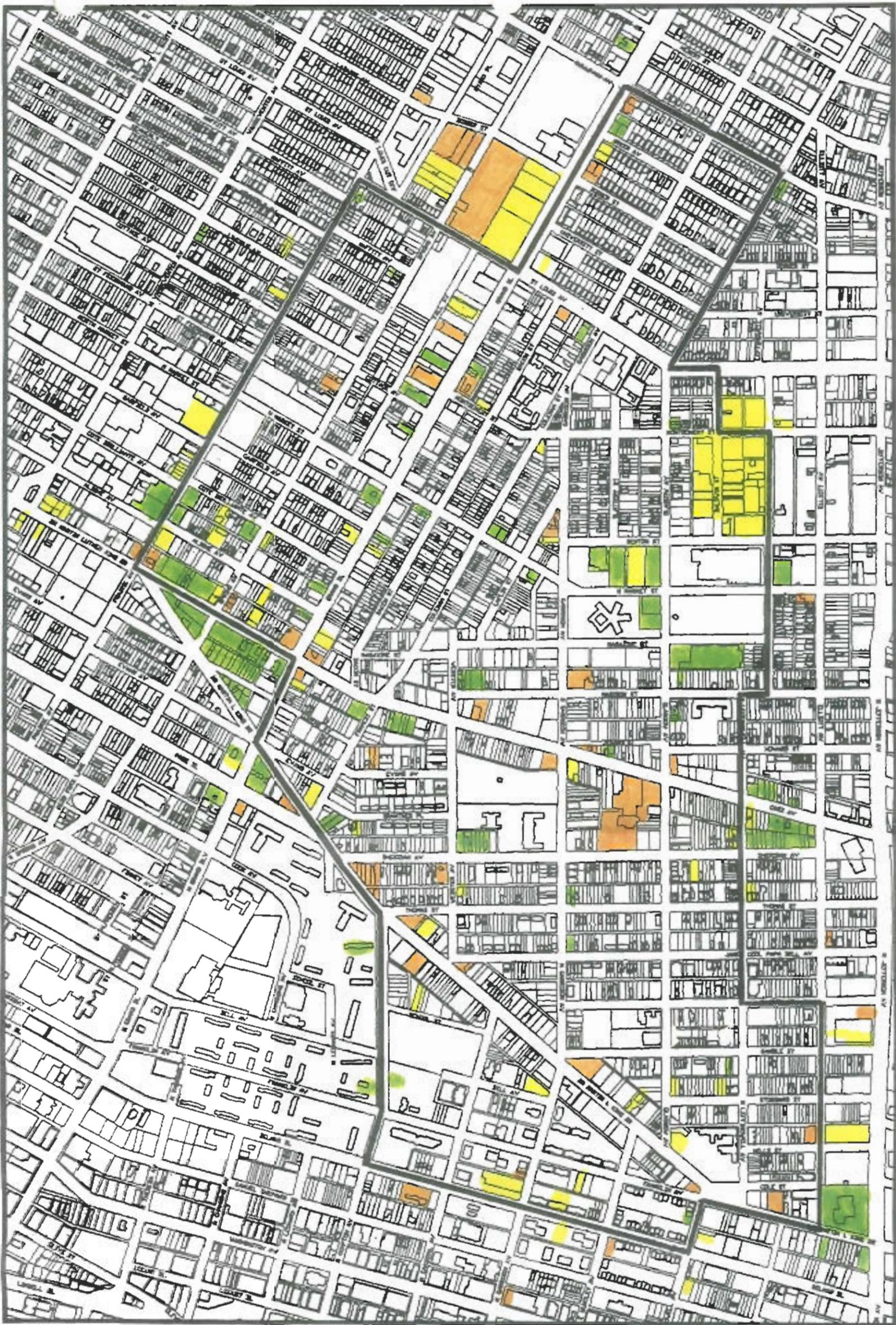
Parks and Open Space: Both plans call for the provision of more open space, more green areas and parks, schools and community centers. It is interesting to note that the 1947 Plan for Public Recreation Facilities describes design standards for neighborhood parks. The principles closely resemble the Vashon High School/JeffVanderLou plan for community interaction. The plan designates approximately twenty acres for a neighborhood park near the center of each residential neighborhood. The elementary school serving the neighborhood would adjoin or be in the park. The school would be the neighborhood community center.

Street Changes: The 1947 plan originally called for the widening of Jefferson Avenue between Franklin and Natural Bridge, the proposed widening of Sheridan Avenue between Jefferson and Franklin, and the proposed widening of Spring Avenue between Page and Natural Bridge. These routes were regarded as "dominant structural elements of the street plan" having unusually high volumes of traffic. The JVL Master Plan calls for less ambitious

street changes including the extension of some small streets and aesthetic improvements such as medians and bulb-outs.

Spot Zoning: Finally, it is also important to note that the City Plan recognizes the need to discourage spot zoning. In fact, a clause was inserted in each dwelling district regulation in order to avoid such an act, with certain exceptions. Spot zoning designates a use classification to an isolated parcel of land incompatible with surrounding uses. In both of our suggested rezoning options, we have agreed with the notion that spot zoning is to be avoided.

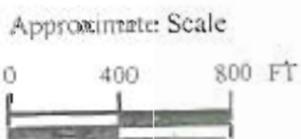
11" x 17" Maps



Notes:

Map features (buildings, roads, property lines, etc.) are taken from the "Vashon High School Catchment Area" drawing, dated 06/24/99, by David Mason & Associates, Inc.

Colored areas are approximate locations of environmental conditions based on field reconnaissance, Sanborn historical maps, and city directories.



LEGEND

-  Study Area Boundary
-  Recognized Environmental Condition
-  Probable Environmental Condition
-  Possible Environmental Condition

(Refer to Tables 1, 2, & 3 for site names and addresses of colored areas)

FIGURE 2

ENVIRONMENTAL CONCERN MAP
Vashon/VL Initiative Study Area
St. Louis, Missouri

Prepared For
McCormack Baron & Associates

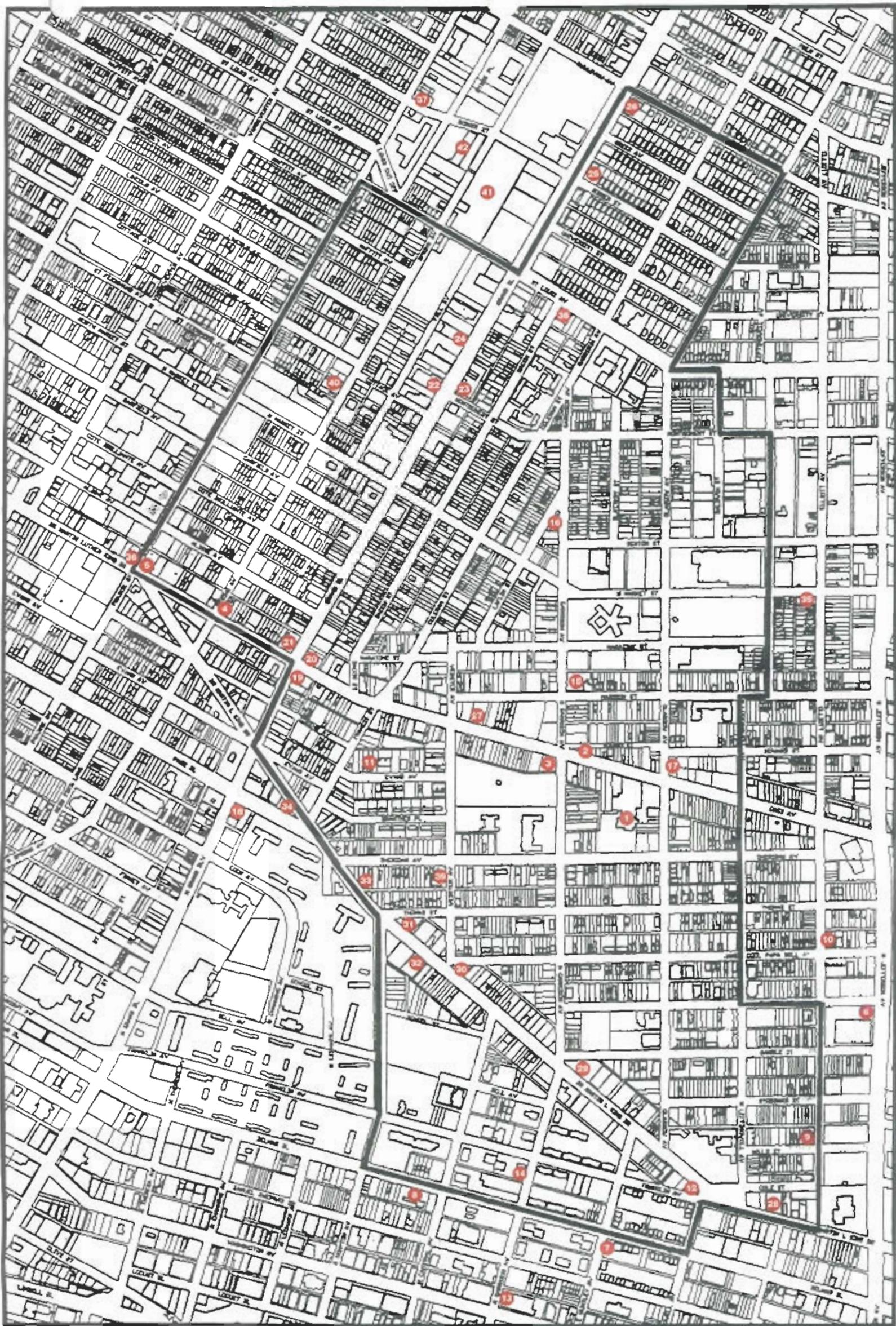


FIGURE 3

RECOGNIZED ENVIRONMENTAL CONDITIONS MAP
 Vashon/JVL Initiative Study Area
 St. Louis, Missouri

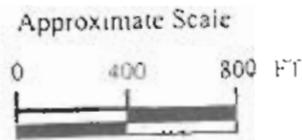
Prepared For
 McCormack Baron & Associates

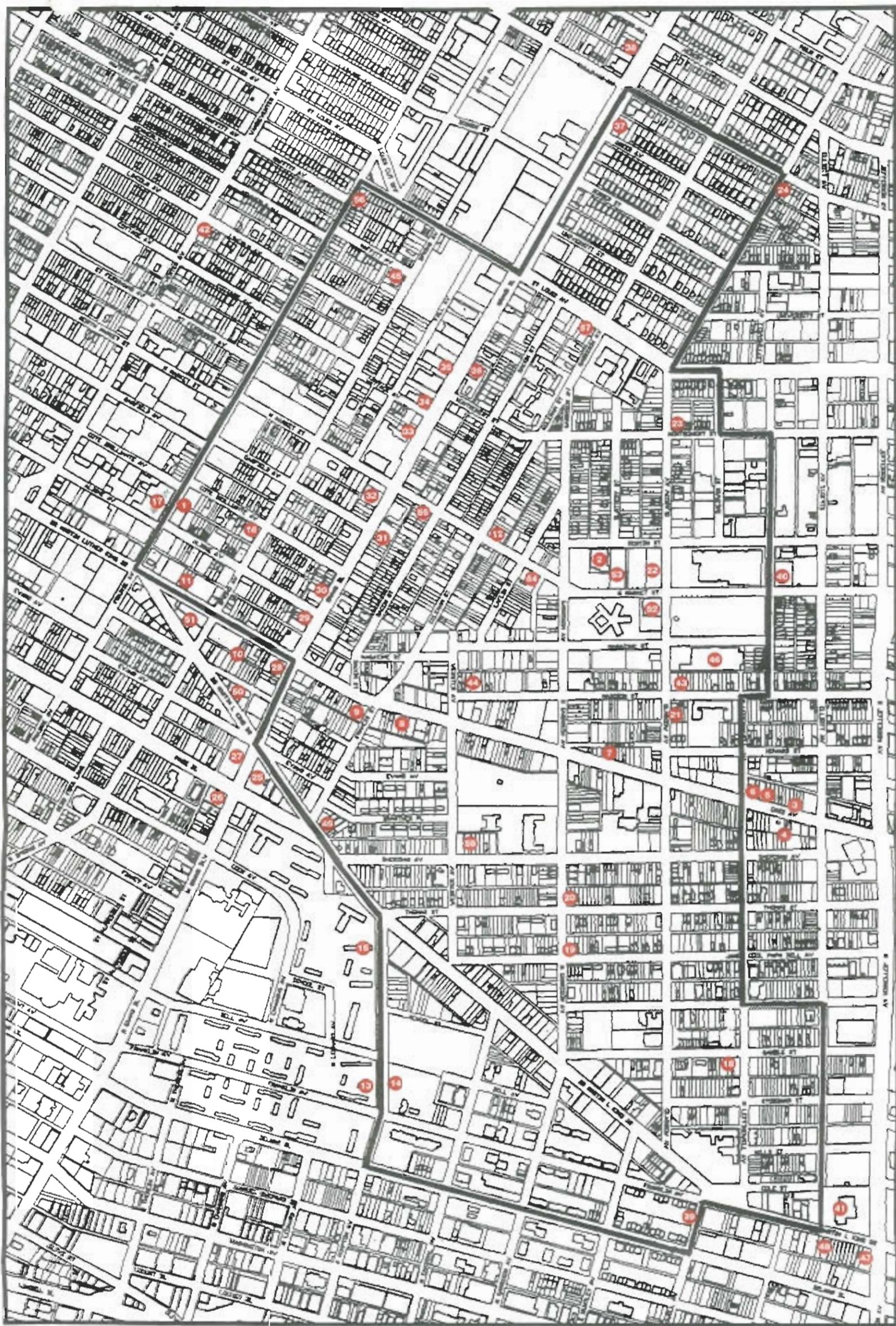
Notes:

Map features (buildings, roads, property lines, etc.) are taken from the "Vashon High School Catchment Area" drawing, dated 06/24/99, by David Mason & Associates, Inc.

Numbered areas above are approximate locations of facilities with recognized environmental conditions based on field reconnaissance, Sanborn historical maps, and city directories. Refer to Table 1 for site names and addresses of above locations.

A recognized environmental condition is defined by ASTM Standard E 1527-97, as the presence or likely presence of a hazardous substance or petroleum product released on site.





Notes:

Map features (buildings, roads, property lines, etc.) are taken from the "Vashon High School Catchment Area" drawing, dated 06/24/99, by David Mason & Associates, Inc.

Numbered areas are approximate locations of facilities with probable environmental conditions based on field reconnaissance, Sanborn historical maps, and city directories. Refer to Table 2 for site names and addresses of above locations.

A probable environmental condition is defined by Professional Environmental Engineers, Inc. (PEE), as the likely presence of a hazardous substance or petroleum product being released on site based on the known data reviewed by PEE. It should be noted that the limitations of this investigation make it entirely possible that recognized environmental conditions, as defined by ASTM Standard E 1527-97, are not present. Additional investigations (site reconnaissance) should be conducted on these facilities to determine if recognized environmental conditions exist.

Approximate Scale

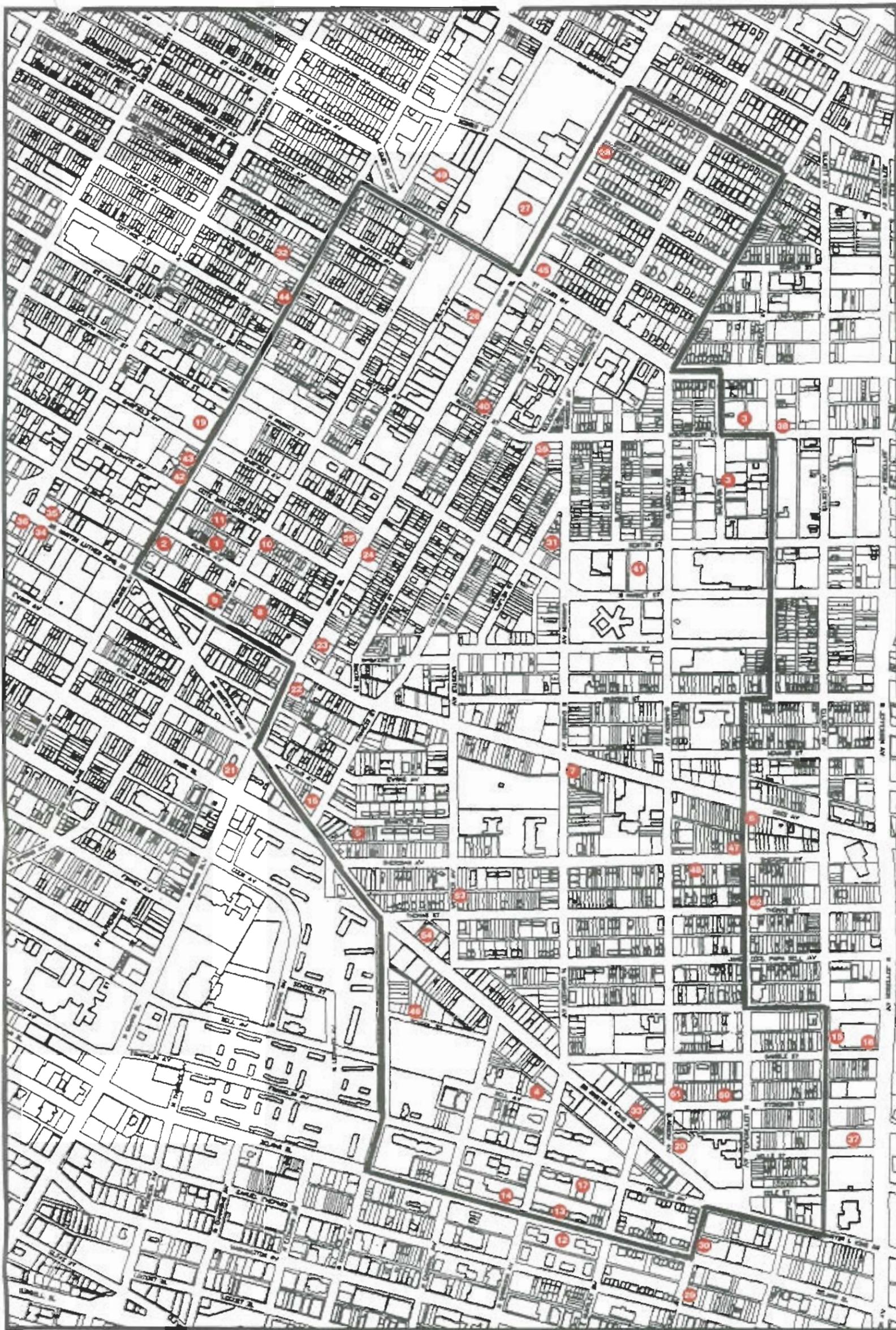


FIGURE 4

PROBABLE ENVIRONMENTAL CONDITIONS MAP

Vashon OVI Initiative Study Area
St. Louis, Missouri

Prepared For
McCormack Baron & Associates



Notes:

Map features (buildings, roads, property lines, etc.) are taken from the "Vashon High School Catchment Area" drawing, dated 06/24/99, by David Mason & Associates, Inc.

Numbered areas are approximate locations of facilities with possible environmental conditions based on field reconnaissance, Sanborn historical maps, and city directories. Refer to Table 3 for site names and addresses of above locations.

A possible environmental condition is defined by Professional Environmental Engineers, Inc., as the possibility of a hazardous substance or petroleum product being released on site. Additional investigations (site reconnaissance) should be conducted on these facilities to determine if recognized environmental conditions, as defined by ASTM Standard E. 1527-97, are present.

Approximate Scale

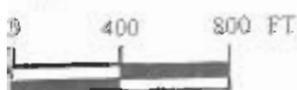


FIGURE 5

POSSIBLE ENVIRONMENTAL CONDITIONS MAP

Vashon/JVL Initiative Study Area
St. Louis, Missouri

Prepared For
McCormack Baron & Associates





The Eastern Edge

- Institutional
- Park
- New Residential
- Existing Residential
- Commercial
- Mixed Use
- Industrial



Dr. Martin Luther King Jr. Drive

- Institutional
- Park
- New Residential
- Existing Residential
- Commercial
- Mixed Use
- Industrial

Vashon/JeffVanderLou Initiative

St. Louis, Missouri

URBAN DESIGN ASSOCIATES
David Mason & Associates

October 2000



Infill South of Cass Avenue

- Institutional
- Park
- New Residential
- Existing Residential
- Commercial
- Mixed Use
- Industrial

Vashon/JeffVanderLou Initiative
 St. Louis, Missouri

URBAN DESIGN ASSOCIATES
 David Mason & Associates
 October 2009



Thematic & Historic Cultural District

- Institutional
- Park
- New Residential
- Existing Residential
- Commercial
- Mixed Use
- Industrial

Vashon/JeffVanderLou Initiative
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